



Subarea Design Guidelines

AREA A

66

AREA B

116

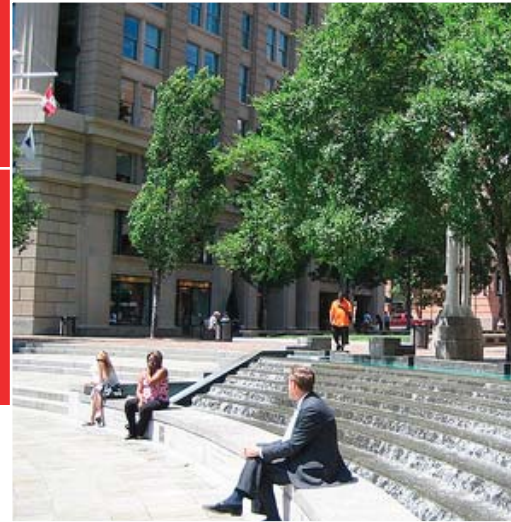
Willets Point is envisioned as a sustainable district that comprises a mixed-use entertainment and dining area (Area A) and a quiet residential neighborhood and convention center with regional access (Area B).

3. Subarea Design Guidelines:

AREA A

Area A guidelines are organized into the following subsections:

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Area A is envisioned as a sustainable, pedestrian-oriented, mixed-use, 24-hour destination with immediate subway access that brings local residents and regional visitors to a diverse mix of retail, entertainment and dining establishments enlivened by festive public spaces.

Situated primarily outside of the restricted height zone, Area A is imagined as an urban redevelopment with iconic high-quality architecture and a highly visible varied skyline that defines a memorable character for Willets Point.

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126th Street

126th Street forms an iconic destination and gateway for Willets Point. A dining, entertainment and retail district complements Citi Field and defines a strong identity with dramatic architecture and spectacular signage. A "Linear Plaza" creates spaces for outdoor dining and gathering and mitigates the floodplain.

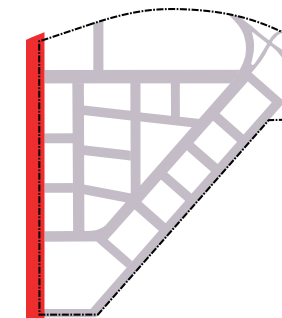


Figure 1. Diagram of 126th Street

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3.1 126TH STREET Overview

THE FACE OF THE DISTRICT

126th Street will be the iconic gateway and first impression of Willets Point for many visitors. It is envisioned as a major regional public destination that includes a Linear Plaza along the entire length of 126th Street connecting Flushing Meadows Corona Park to the Flushing Bay and mixed-use development with a strong street-level focus on dining and entertainment establishments. This development can both leverage the presence of Citi Field patrons and also provide attractions year-round on non-game days.

The Linear Plaza is not only a major public space that enhances the retail district but also a means of creating a graceful grade change between the existing grade of 126th Street and the elevated ground floors which must be built above the FEMA 100-year flood elevation.

126th Street will be divided into at least five building parcels due to intersection locations required by the SZD intended to create a walkable district. At both ends of the street, “anchor” parcels form gateways that are visible from the elevated subway line and the Whitestone Expressway. A Two-Block Core opposite Citi Field can become a focal point for 126th Street.

All elevation grades in this document are in feet NGVD29 with a FEMA 100yr floodplain of 14 feet NGVD29.

THE CORE OF 126TH STREET

The “Two-Block Core” opposite Citi Field is the highlight of 126th Street and should be distinctively expressed in both the streetscape and architecture to create a cohesive urban space relating to the stadium.

These two blocks create a central hub of dining and entertainment uses that builds on a direct relationship to the stadium, creating an “urban room”. The buildings are allowed additional setbacks in order to create second-floor dining terraces overlooking the stadium entrance. The streetscape on these two blocks relates to the stadium and creates spaces for outdoor dining and gathering. See Figures 2 and 3.

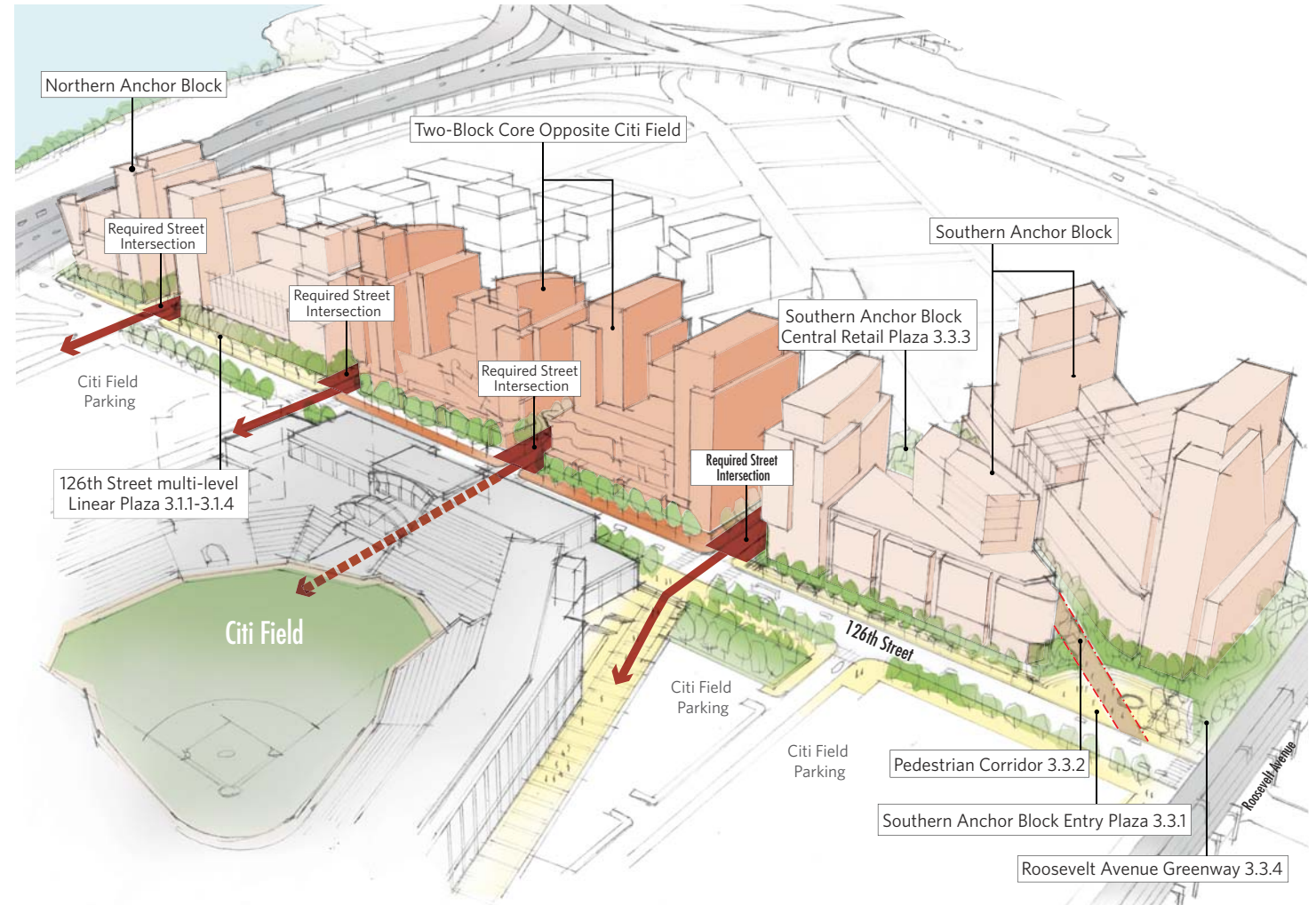
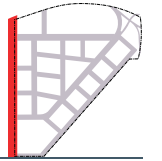


Figure 2. Diagrammatic Isometric View of 126th Street
The above image depicts the intent of the design guidelines and is for illustrative purposes only



PUBLIC REALM GUIDELINES

See pages 78–79 for details.

The unique streetscape of 126th Street transforms the potential negative condition of a grade change for flood protection into a positive – by creating a split-level Lower Sidewalk and Upper Linear Plaza.

- 3.1.1 Lower Sidewalk: Part of the NYC Public Street System**
- 3.1.2 Transition Zone: Graceful Grade Change**
- 3.1.3 Upper Zone: Outdoor Dining, Shopping and Strolling**
- 3.1.4 Open and Inviting Corner Treatments**

ARCHITECTURAL GUIDELINES

See pages 80–91 for details.

The mixed-use buildings of 126th Street are intended to have destination entertainment, dining and retail uses catering to both day-time and night-time visitors with residential and commercial towers above. Upper Base facades are enlivened by spectacular signage or art installations, above which towers frame gateways into the site and define a dynamic skyline for the district.

- 3.1.5 Two-Block Core Opposite Citi Field**
- 3.1.6 Emphasize Verticality of Towers**
- 3.1.7 Respond to Citi Field Massing**
- 3.1.8 Gateways at Connector Streets**
- 3.1.9 Spectacular Facade Features that Screen Parking**

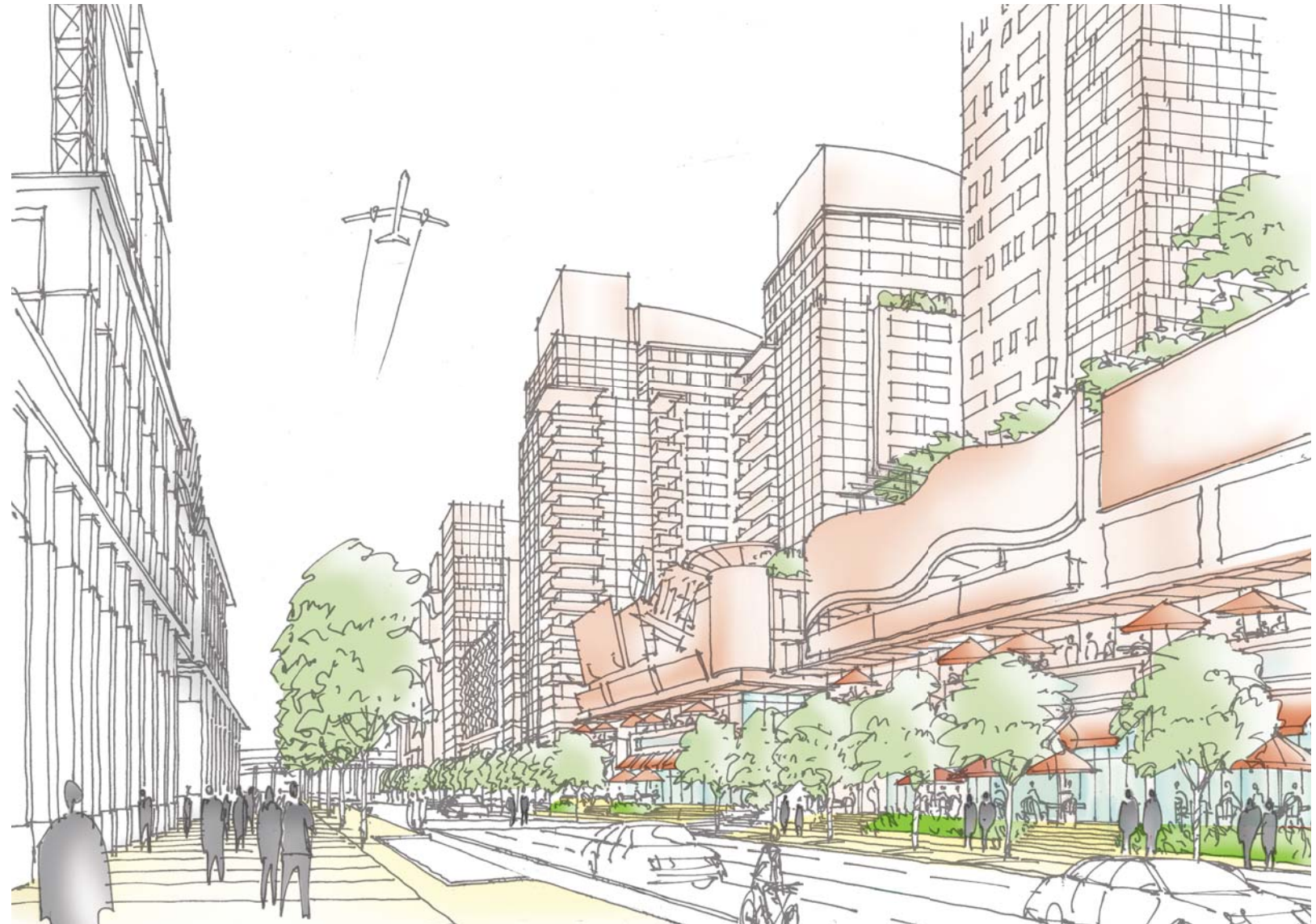


Figure 3. Illustrative view depicting the streetscape and the architectural expression of the buildings on the Two-Block Core opposite Citi Field
The above image depicts the intent of the design guidelines and is for illustrative purposes only

3.1 126TH STREET

THE 126TH STREET LINEAR PLAZA

126th Street will have a unique streetscape character resulting from its unusual physical design requirements. As shown in *Figure 5*, while the existing street elevations range from approximately 10' to 13' above mean sea level (AMSL), the ground floor entrances to new buildings are suggested to be built above the FEMA 100-year flood plain elevation of +14.0' AMSL. This results in a height difference ranging from approximately 1' to 4' between the sidewalk and the new building ground floors. At Citi Field, the ground floor retail spaces use flood gates and are accessed directly at the grade of 126th Street while the playing field is raised and accessed through a series of steps and other means.

To accommodate this difference in grades, and ensure that retail tenants will be successful with sufficient visibility and street access, the SZD requires a setback of new buildings from the existing curb. This setback allows for a potential split-level pedestrian plaza that creates a graceful grade change and provide ample public areas fronting retail establishments. At the lower level, a New York City sidewalk runs consistently along the curb. Since the Willets Point property line is actually at the existing curb line, the entire sidewalk lies within the Willets Point development area. See *Figure 5*.

COMPONENTS OF THE LINEAR PLAZA

Type 1: Linear Landscape

Linear Landscape is a streetscape character defined by long stretches of landscape. It is envisioned as a split-level pedestrian open space and is the predominant character along 126th Street. Primarily fronting retail establishments, the Linear Landscape should facilitate access to storefronts, provide a clear walkway, seating areas and potential sustainable stormwater management. See *Figure 4*.

Type 2: Two-Block Core

The Two-Block Core opposite Citi Field creates the focal point for the Linear Plaza along 126th Street. While still following New York City standards, the streetscape should create a well-defined "urban room" that relates to the stadium streetscape across the street. Primarily fronting restaurants and entertainment venues, these blocks should be primarily hardscape with formal and informal outdoor seating with pockets of landscape. See *Figure 4*.

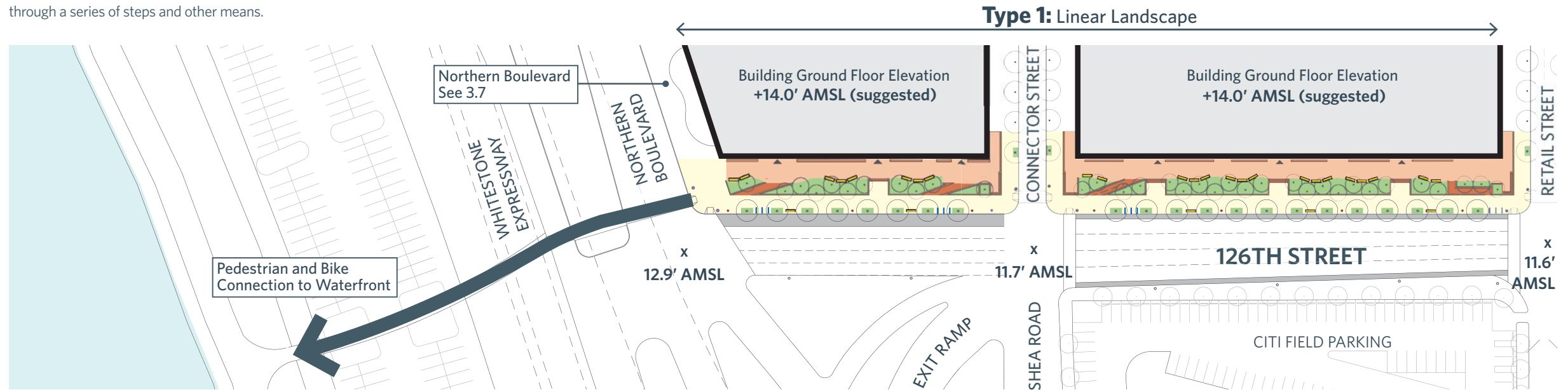


Figure 4. Plan of 126th Street depicting the two components of the Linear Plaza along 126th Street
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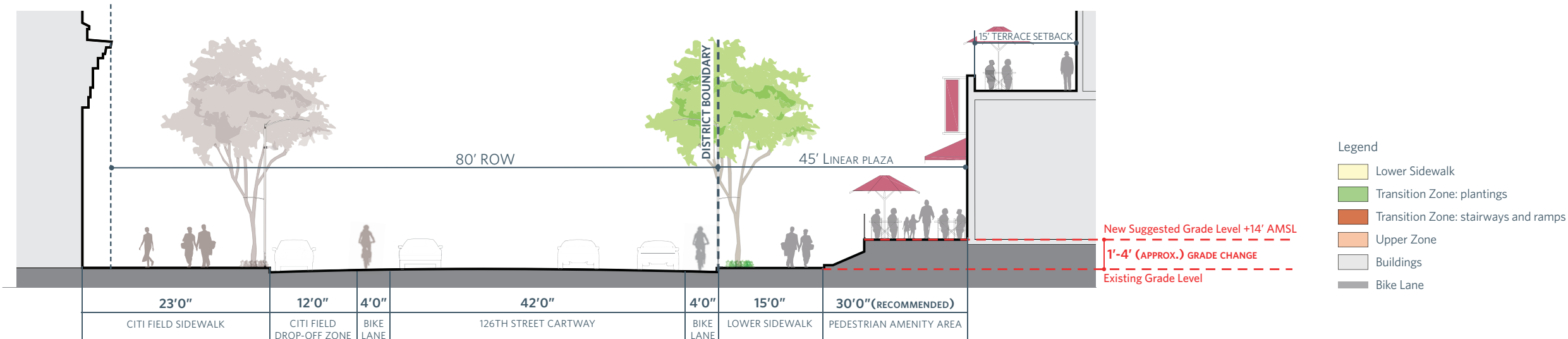
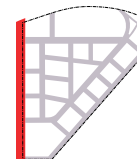
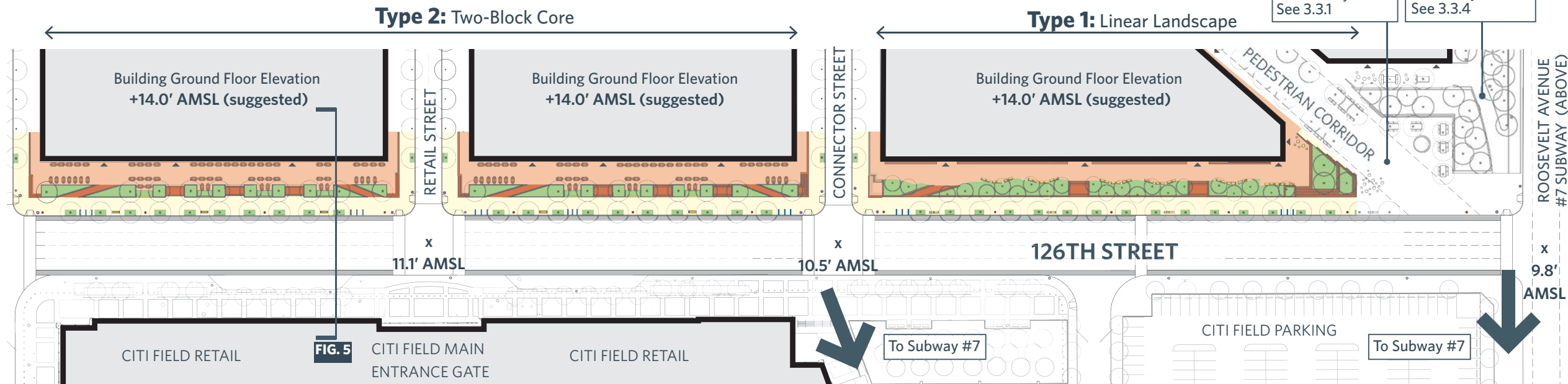


Figure 5. Street section of 126th Street depicting the grade change condition and street ROW.
 NOTE: The actual layout of the street will have to be developed in concert with the New York City Department of Transportation.



3.1 126TH STREET

LINEAR PLAZA — LOWER SIDEWALK

Type 1: Linear Landscape

View of the Lower Sidewalk

Legend

- Lower Sidewalk
- Transition Zone: plantings
- Transition Zone: stairways and ramps
- Retail Entrance



Include sloping, naturalistic landscapes with native plantings and potential stormwater management features in the Transition Zone

Create graceful monumental steps to encounter grade change

Include native plantings and potential stormwater management features in the tree pits

Design the Lower Sidewalk as part of the NYC public street system

Figure 6. Type 1 Linear Landscapes: Illustrative sketch depicting the lower sidewalk and transition treatments
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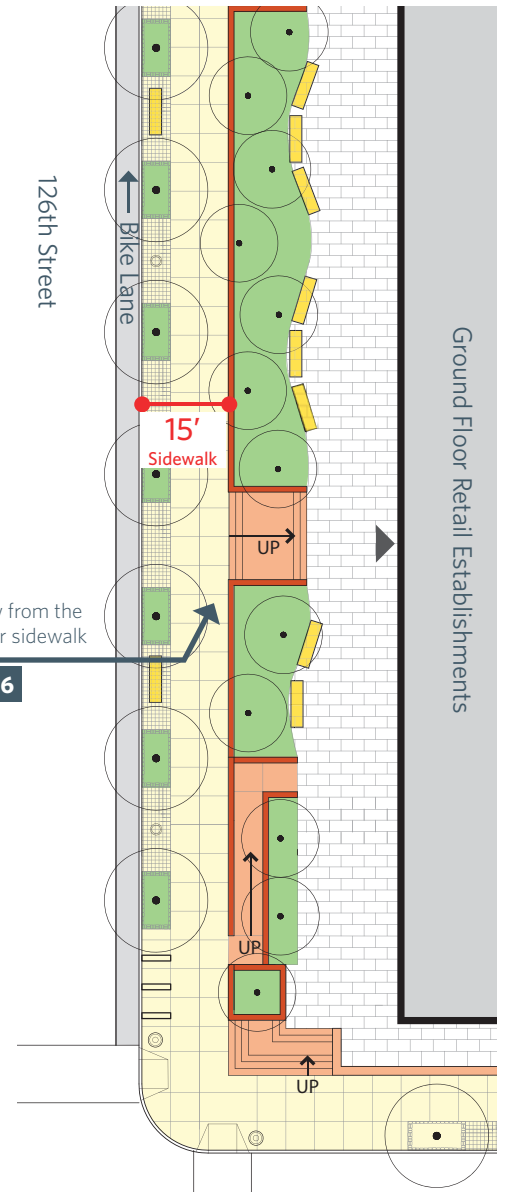


Fig. 6



Figure 7. Type 1 Linear Landscape Lower Sidewalk and Transition Zone



LINEAR PLAZA — LOWER SIDEWALK

Type 2: Two-Block Core

View of the Lower Sidewalk

Legend

- Lower Sidewalk
- Transition Zone: plantings
- Transition Zone: stairways and ramps
- Retail Entrance

Potential second-floor dining terraces that relate to the stadium

Design frequent stair systems that can function as informal seating

Include seat walls at the edges of planted areas to encourage social seating in the Transition Zone

Follow standard NYCDPR designs for tree pits

Design the Lower Sidewalk system as part of the NYC public street with design features to highlight the Two-Block Core



Figure 8. Two-Block Core: Illustrative sketch depicting the lower sidewalk and transition treatments
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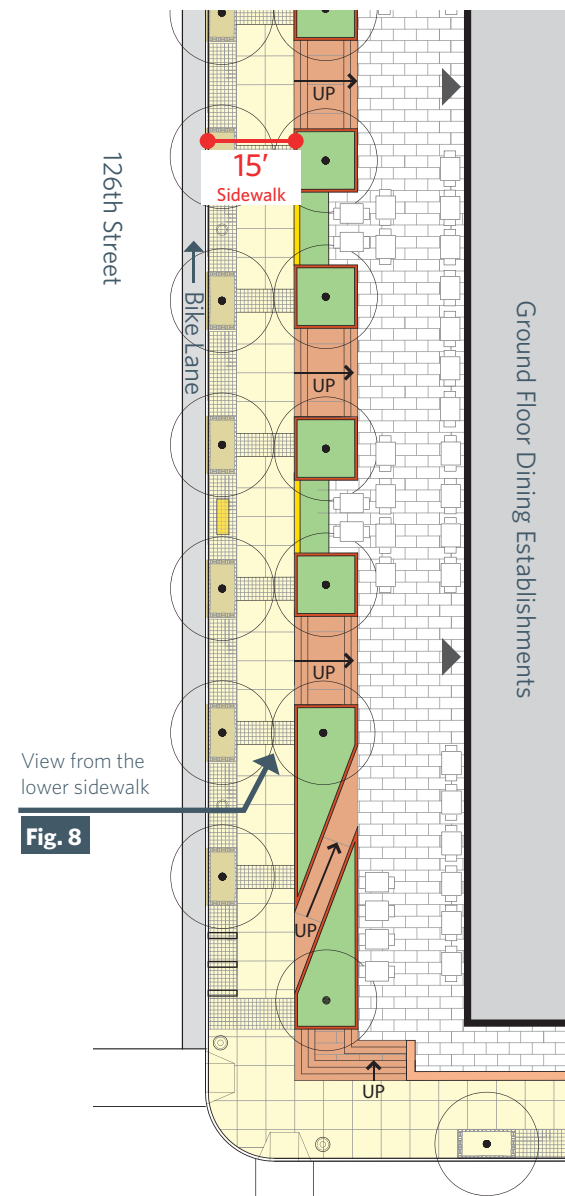


Fig. 8



Figure 9. Two-Block Core Lower Sidewalk and Transition Zone

3.1 126TH STREET

LINEAR PLAZA — UPPER ZONE Type 1: Linear Landscape

View of the Upper Zone

Legend

- Transition Zone: plantings
- Upper Zone
- Retail Entrance



Include green areas that undulate in form providing a variety of spaces and edge conditions in the Upper Zone

Create social seating nodes at the edge of the planted area

Provide clear pedestrian movement areas adjacent to the retail establishments to allow for ease in shopping and strolling

Demarcate a zone in front of the retail streetwall, used for outdoor merchandise displays

Figure 10. Type 1 Linear Landscapes: Illustrative sketch depicting the Upper Zone
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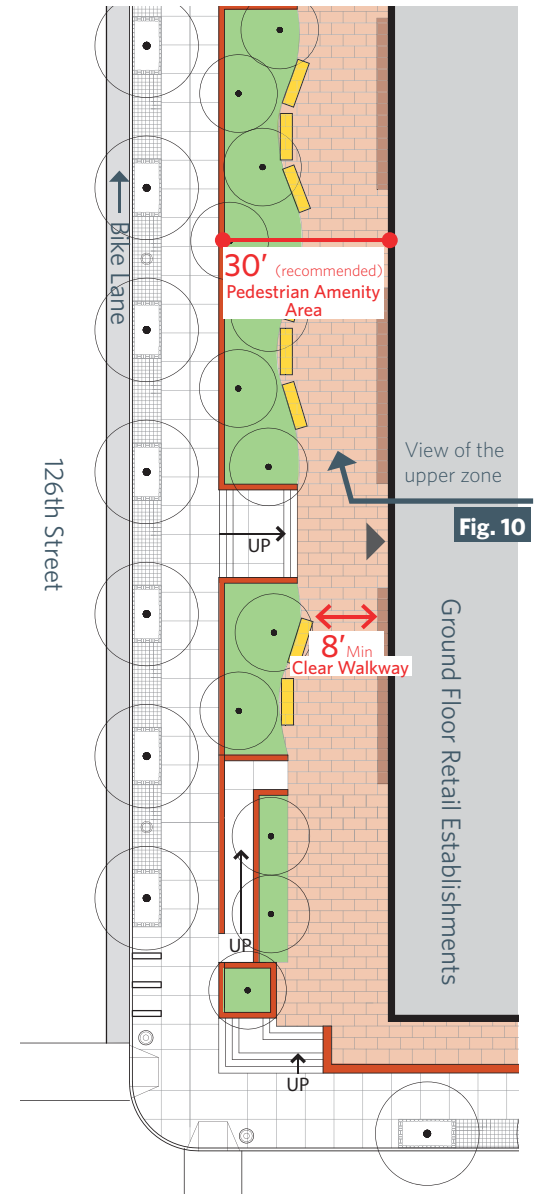


Fig. 10

Figure 11. Type 1 Linear Landscape Transition and Upper Zone plan

LINEAR PLAZA — UPPER ZONE Type 2: Two-Block Core

View of the Upper Zone

Legend

- Transition Zone: plantings
- Upper Zone
- Retail Entrance



Allow for clear and continuous pedestrian walkways within the outdoor dining areas

Locate plantings on the Upper Zone in regular intervals to define niches for outdoor dining

Demarcate a zone in front of the retail streetwall, used for outdoor merchandise displays

Figure 12. Type 2 Two-Block Core: Illustrative sketch depicting the Upper Zone
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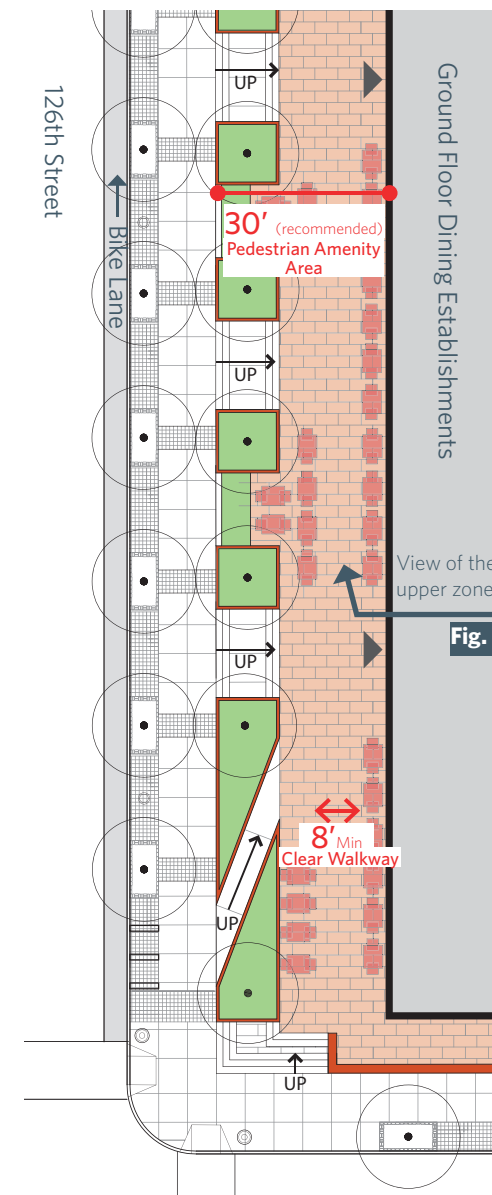


Fig. 12



Figure 13. Two-Block Core Transition and Upper Zone Plan

3.1 126TH STREET Public Realm Guidelines

For Guidelines regarding issues common to all streetscapes and open spaces, see Chapter 2 Design Quality Standards: Public Realm.

3.1.1 Lower Sidewalk: Part of the NYC Public Street System

Although located inside of the property line of Willets Point, the Lower Sidewalk should form a seamless and continuous pedestrian link between all adjacent areas outside of the district. They should be reflective of the vocabulary and appearance of a typical NYC public street.

- Design and locate streetscape elements to permit access between the curb and street at least once at every tree interval. See *Figure 14*.
- Align access between the roadway and Lower Sidewalk with Upper Zone access points and storefront entries to create direct access between the roadway and the storefront entries. See *Figure 14*.
- Coordinate landscaping between Lower Sidewalk tree pits and planting beds in Transition Zone and Upper Zone.



The lower sidewalk should look and feel like a high-quality New York City public sidewalk

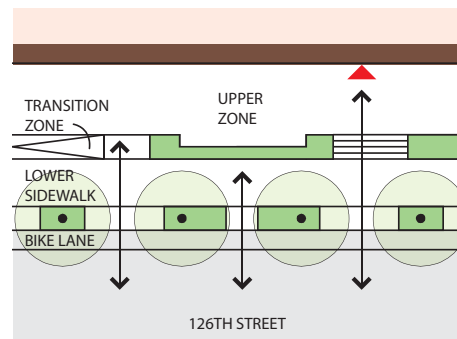


Figure 14. Diagram of access points between the roadway, Lower Sidewalk, Upper Zone and storefront entries.

3.1.2 Transition Zone: Graceful Grade Change

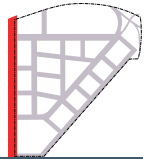
The Transition Zone refers to the grade change zone of the area called the "Pedestrian Amenity Area" in the SZD. The transition between the Lower Sidewalk and the Upper Zone should include landscaping, promote movement between the levels and provide informal seating and gathering areas.

- Include stairs, ramps, seating walls, terraced plantings, and possibly public art in the Transition Zone. See *Figures 6-9*.
- Per the SZD, Transition Zones that are a part of the 20'-35' wide "Pedestrian Amenity Area" must include a minimum of two ADA-compliant ramps on each block.
- Coordinate the Transition Zone with outdoor seating on the Upper Zone and provide easy and immediate access to each retail establishment.
- Incorporate social seating nodes and generous open stair systems into the Transition Zone.
- Avoid plinth walls over 18" in height. Walls can be softened by terracing and landscape.
- It is recommended that 50% of the Transition Zone be green.
- 1 tree per 40 linear feet is recommended, grouped in massings or naturally arranged, in the Transition Zone or the Upper Zone.

Note: All Publicly Accessible Open Space Requirements per SZD are applicable.



Transition treatments include stairs and terraced plantings that might be used for seating



3.1.3 Upper Zone: Outdoor Dining, Shopping and Strolling

The Upper Zone refers to the upper walkway of the area called the “Pedestrian Amenity Area” in the SZD. The design of this zone is vital to the viability of retail.

- In order to give the Linear Plaza a unique character, consider using a distinctive palette of streetscape furniture, lighting, paving, and other elements here, as compared to the more standard NYC palette suggested for the Lower Sidewalk.
- Incorporate social seating areas into the Upper Zone.
- Design areas of cafe dining to allow for continuous pedestrian movement along the Upper Zone walkway.
- Per the SZD, the pedestrian circulation space at the Upper Zone is required to be a minimum of 5' wide; however at least 8' wide is recommended.
- Design planted areas to undulate in form providing a variety of spaces.
- 1 tree per 40 linear feet is recommended, grouped in massings or naturally arranged, in the Transition Zone or the Upper Zone.
- Provide shaded seating at the Upper Zone.

Note: All Publicly Accessible Open Space Requirements per SZD are applicable.



Walkway for shopping, Chicago



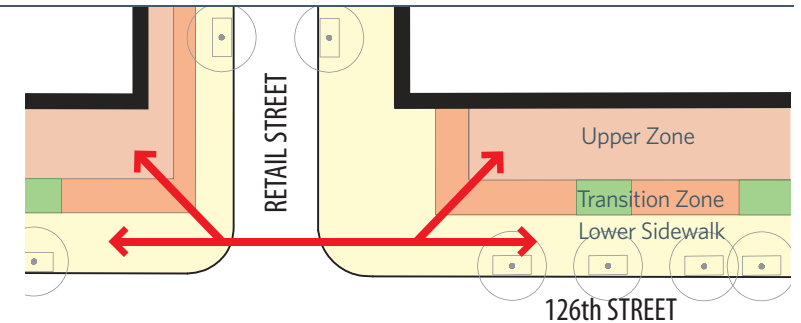
Walkway for dining, Miami

3.1.4 Open and Inviting Corner Treatments

Corners facilitate the movement of people between blocks and between the upper and lower levels and should be designed to create strong physical and visual connections.

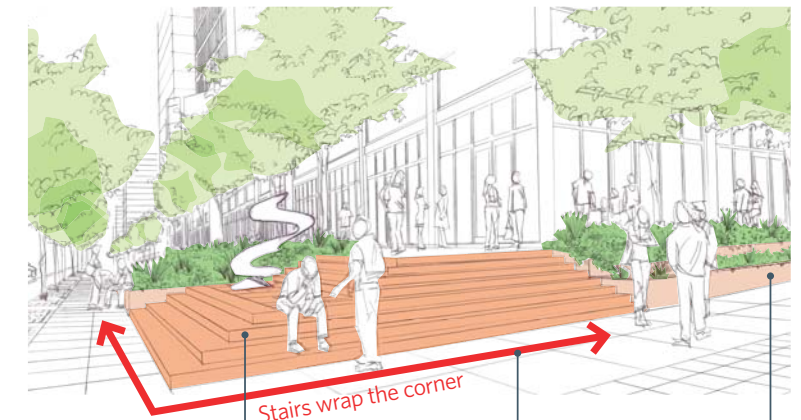
- Consider including public art and social seating at corners, especially at the intersections of highly visible, important streets.
- Design corner treatments to maximize access between the lower and upper levels. For example, stairs that wrap the corner or stairs that are pulled away from the corner allowing the retail to reach the lower level, are two possible configurations. See Figure 15.
- Avoid plinth walls and plantings that restrict movement. See Figure 16.
- Coordinate corner treatments with storefronts and outdoor seating on the Upper Zone.

Note: All Publicly Accessible Open Space Requirements per SZD are applicable.



Design corners to provide easy access between the Lower Sidewalk and the Upper Zone and facilitate a continuous path along the Lower Sidewalk

PREFERRED CORNER TREATMENT:



Provide open stairs that function as informal seating at corners

Design corners to allow for easy movement between the upper and lower levels

Avoid plinth walls by treating them with plantings, water features, terraces, or benches

Figure 15. Open and inviting corners with steps designed as informal seating areas
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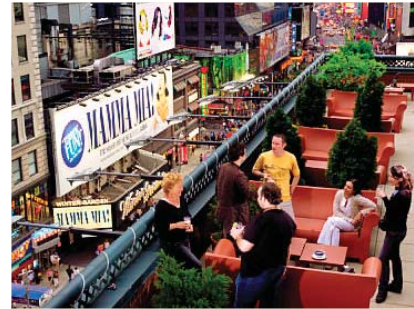
3.1 126TH STREET Architectural Guidelines

For Guidelines regarding issues common to all building facades, see Chapter 2 Design Quality Standards: Architecture.

3.1.5 Two-Block Core Opposite Citi Field

The Two-Block Core opposite Citi Field is the only area of 126th Street with retail on both sides. In concert with stadium retail, it creates the focal point for 126th Street.

- Locate destination dining and entertainment establishments such as restaurants, live music venues, bars, and clubs with outdoor seating, to face restaurants within the Citi Field building. See Figure 16.
- As allowed in the SZD, there is an opportunity on these two blocks to provide an additional setback with a minimum depth of 15' on the second floor to encourage eating and drinking establishments to have outdoor seating terraces overlooking 126th street and the stadium. See Figure 16.
- To further define this Two-Block Core, align Priority Towers with the northern and the southern edges of the stadium building, to spatially define an "urban room". See Figure 17.



Active upper level restaurant terraces engage and add to the vibrant character of the street

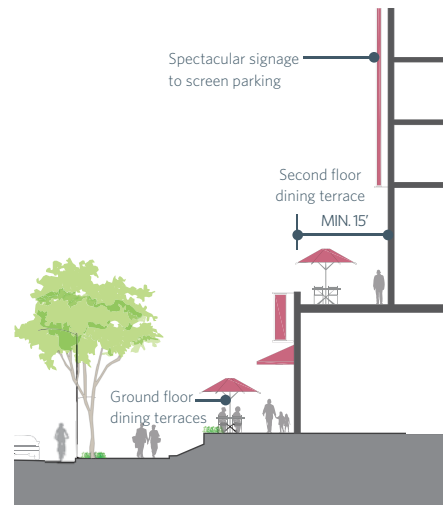


Figure 16. Section depicting the second floor dining terrace setback on the two block core opposite Citi Field

3.1.6 Respond to Citi Field Massing

The buildings along 126th Street should respond to the heights established by Citi Field. See Figure 17.

- Design the base heights of buildings along 126th Street to relate to the base portion of the Citi Field building that is approximately 85'.
- The maximum allowable height of towers (+232' AMSL) is determined by the top of the score board of the stadium, per FAA and PANYNJ regulations.

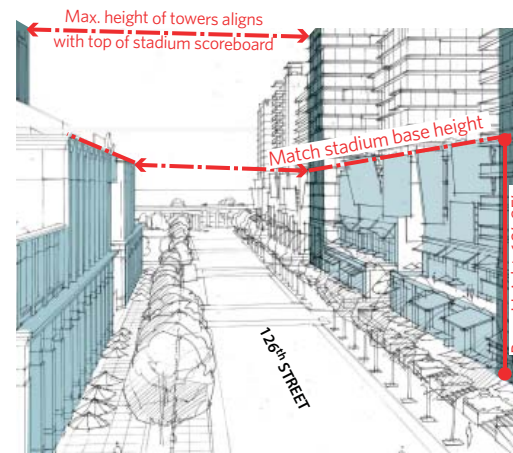


Figure 17. Buildings along 126th Street should respond to the massing of the Citi Field Stadium

3.1.7 Gateways at Connector Streets

Architecture can create gateways at the intersections of 126th Street and Connector Streets to demarcate major entries into Willets Point. See Figure 18.

- Locate Priority Towers in these locations to frame these intersections.
- Consider using architectural strategies such as glazing, facade articulation and distinctive materials to emphasize the corners of the buildings.

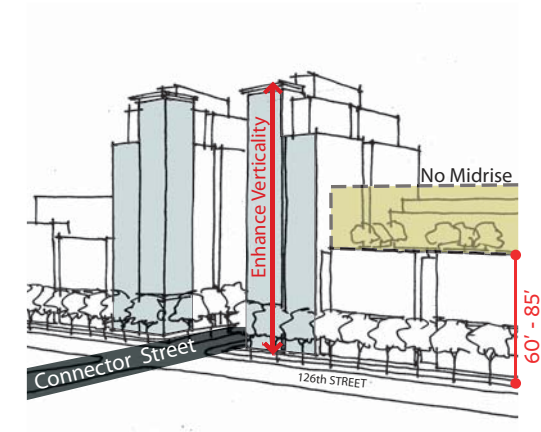
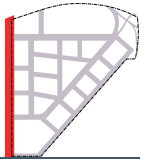


Figure 18. Buildings along 126th Street should avoid mid-rise portions to accentuate verticality of towers and highlight the Connector Street gateways



3.1.8 Emphasize Verticality of Towers

Towers on 126th Street should be designed to emphasize their verticality.

- To achieve this, it is preferred that there is no mid-rise portion of buildings along the block front parallel to 126th Street. See *Figure 18*.
- Design the architecture of the towers to clearly emphasize verticality in their massing and materiality. Consider additional glazing at corners, especially on highly visible towers.
- Setback tower tops, per the SZD, in a manner that emphasizes the vertical expression.

3.1.9 Spectacular Facade Features that Screen Parking

Per the SZD, all buildings along 126th Street except for those on Anchor Blocks are permitted to have parking garages between a height of 35' and 85' at the streetwall which must be concealed with signage, art, or vertical landscape elements. See *Figure 19*.

For further details, see *Chapter 2 Design Quality Standards: Architecture*.

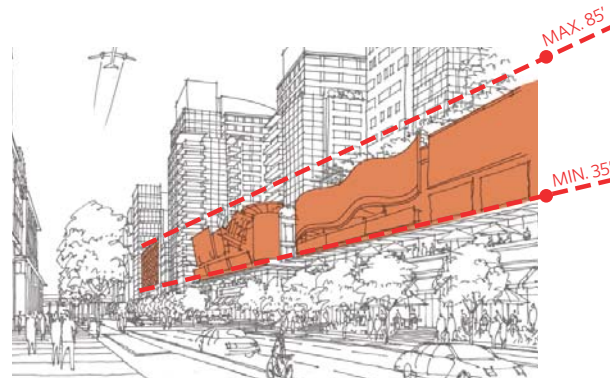


Figure 19. Solutions for screening parking along 126th Street include signage, facade art and green walls

PREFERRED SCREENING OPTIONS:

GREEN WALLS



SPECTACULAR SIGNAGE



ART & LIGHTING



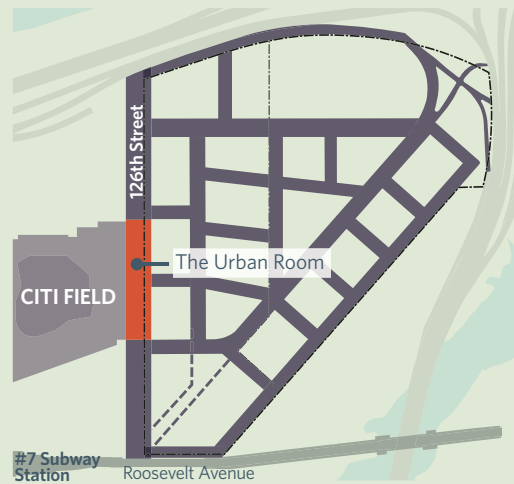
The possibilities for screening the parking should be creative and make distinctive use of materials, lighting, landscape, and other media, contributing to a unique, dynamic, and innovative urban space

3.1 126TH STREET

ENHANCEMENT OPPORTUNITY

The “Urban Room”

Although outside of the district boundary, there is an opportunity to create a unified outdoor urban space between the Two-Block Core and the Citi Field stadium. This “urban room” can be achieved through the use of gateways, banners or lighting that spans the roadway to frame the space and connect the two sides of the street. In addition, distinct paving within the roadway and crosswalks can be used to demarcate this two block zone and create a pedestrian-oriented street.



KEY PLAN



Artistic lighting over the street can unify a streetscape

Retail Streets

The Retail Streets create an urban, pedestrian-oriented, outdoor shopping and mixed-use environment at the core of Willets Point. They accommodate a large amount of retail in a walkable street setting, forming part of the urban fabric of Queens and New York City. Mixed-use towers above retail and concealed parking create a 24-7 vitality for the district.

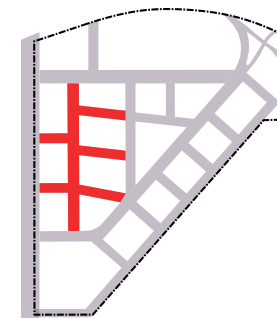


Figure 1. Diagram of the Retail Streets

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3.2 RETAIL STREETS Overview

PEDESTRIAN-ORIENTED RETAIL STREETS

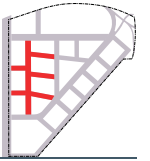
The street grid is centered around the north-south Primary Retail Street with east-west Retail Streets providing connections to 126th Street and the residential neighborhood. The Retail Streets should serve primarily retail district traffic with ample on-street parking to provide visibility and access to retail while maintaining a pedestrian-oriented space. A planted median is allowed per the SZD on the Primary Retail Street that, if provided, will define a unique and memorable character to the street.



Examples of vibrant and successful Retail Streets in mixed-use neighborhoods include SoHo, New York (left) and Portland, Oregon (right)



Figure 2. Diagram of the Retail Streets
The above image depicts the intent of the design guidelines and is for illustrative purposes only



PUBLIC REALM GUIDELINES

See pages 88-89 for details.

The Retail Streets create an intimately-scaled pedestrian-oriented environment for strolling, shopping, and dining.

- 3.2.1** Sidewalk Furnishing Area for Retail Streets
- 3.2.2** Storefront Activity Area
- 3.2.3** Pedestrian Amenity Zone
- 3.2.4** Pedestrian-Oriented Intersections and Crossings
- 3.2.5** Planted Median

ARCHITECTURAL GUIDELINES

See page 90 for details.

Buildings on Retail Streets contribute to a mixed-use environment with active retail storefronts and residential and commercial uses above.

- 3.2.6** Active Retail Lower Base
- 3.2.7** Upper Base Creates Consistent Streetwall
- 3.2.8** Mid-rise Articulated from Upper Base
- 3.2.9** Towers Setback from Mid-rise



Figure 3. Illustrative rendering of the Primary Retail Street

The above image depicts the intent of the design guidelines and is for illustrative purposes only

3.2 RETAIL STREETS

PRIMARY RETAIL STREET

The Primary Retail Street is parallel to 126th Street and is the heart of the retail district. It is a two-sided retail street and is terminated at its northern and southern ends by Anchor Blocks. The pedestrian-oriented street supports a mixture of retail including medium and small stores, boutiques, restaurants, and convenience retail. Multiple storefronts per block front are encouraged.

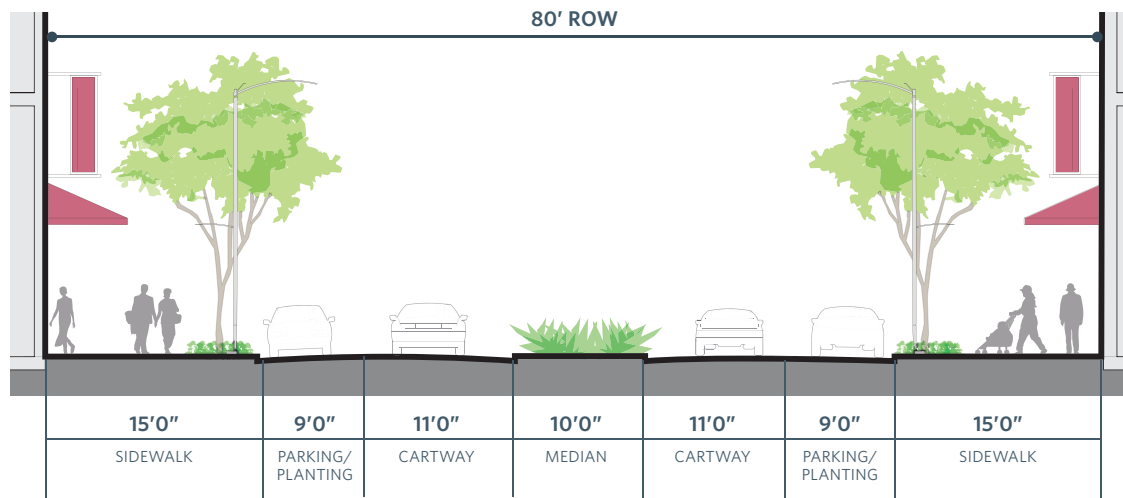
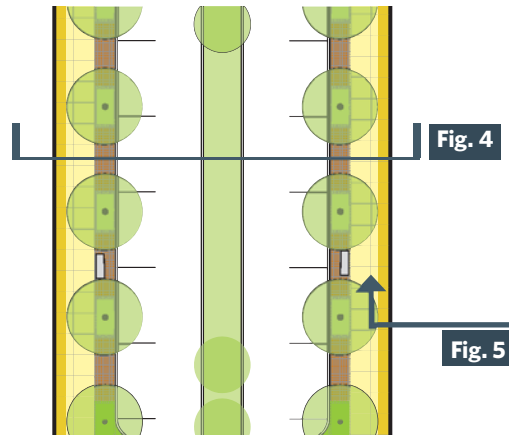
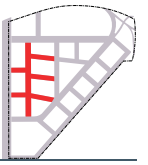


Figure 4. Typical plan and section of the Primary Retail Street
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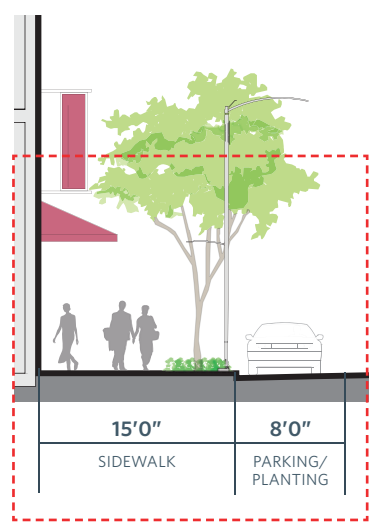
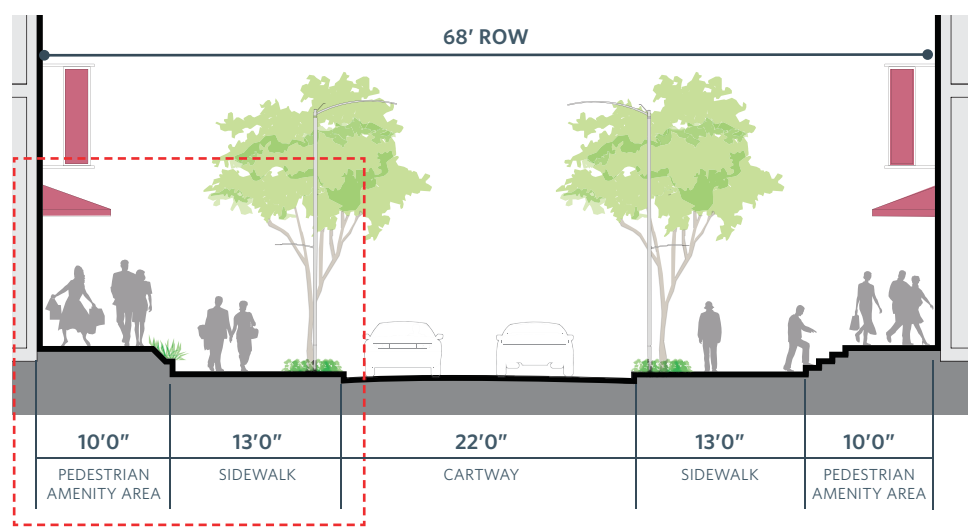
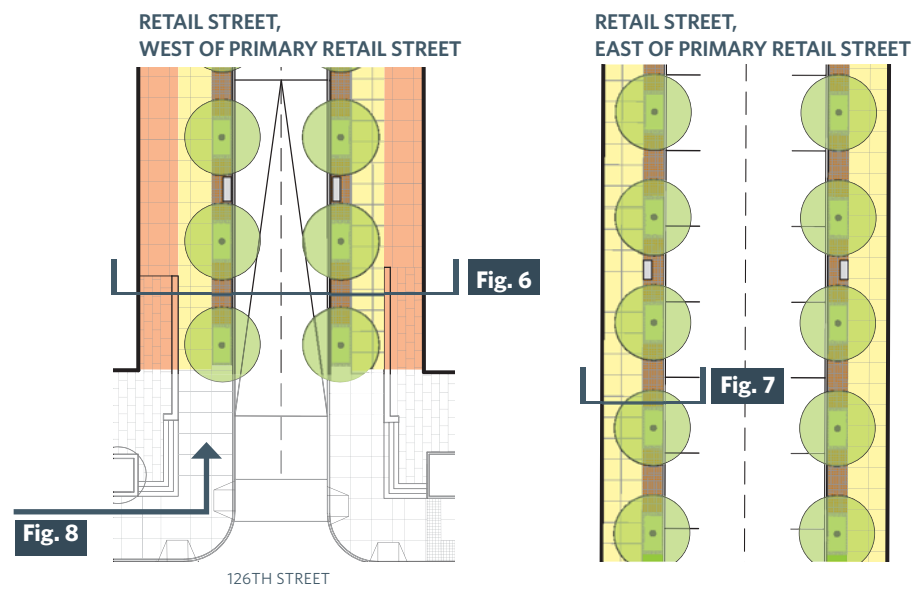
- Provide a planted median along the Primary Retail Street
- Locate public pedestrian amenities in the Sidewalk Furnishing Zone
- Design the sidewalk as part of the NYC street system with ample space for shopping, strolling and sitting
- Provide space for outdoor retail activities on the sidewalk in the Storefront Activity Area

Figure 5. Illustrative sketch depicting the streetscape design of the Primary Retail Street
The above image depicts the intent of the design guidelines and is for illustrative purposes only



RETAIL STREETS

There are two variations of Retail Streets. Streets to the west of Primary Retail Street connect to 126th Street, while streets to the east connect to the residential neighborhood in Area B. Along the western streets, on-street parking has been eliminated to create a widened sidewalk with space for a grade change transition. Eastern streets are considered a transition to the residential neighborhood. All service and parking access will occur on these Retail Streets, rather than the Primary Retail Street, and needs to be integrated into a pedestrian-friendly streetscape.



Design the Pedestrian Amenity Area to accommodate the grade change and connect to 126th Street

On Retail Streets west of Primary Retail Street, there is no on-street parking in order to dedicate space to the split-level sidewalk

Figure 6. Typical plan and section of the Retail Street, west of Primary Retail Street
The above image depicts the intent of the design guidelines and is for illustrative purposes only

Figure 7. Typical section of the Retail Street, east of Primary Retail Street
The above image depicts the intent of the design guidelines and is for illustrative purposes only

Figure 8. Illustrative sketch depicting the streetscape design of the Retail Street, west of the Primary Retail Street
The above image depicts the intent of the design guidelines and is for illustrative purposes only

3.2 RETAIL STREETS Public Realm Guidelines

For Guidelines regarding issues common to all streetscapes and open spaces, see Chapter 2 Design Quality Standards: Public Realm.

3.2.1 Sidewalk Furnishing Area for Retail Streets

The Sidewalk Furnishing Area provides space for plantings and pedestrian amenities between the sidewalk and the road. See Figure 9.

- Along the Primary Retail Street, a minimum of 1 linear foot of seating for every 4 linear feet of sidewalk per block is recommended.
- In order to provide access to the parking lane, provide access between the sidewalk and the street at least once at every tree interval.
- Consider expanding the Sidewalk Furnishing Area in strategic areas to create a more generous pedestrian area. See 3.2.4 Pedestrian-Oriented Intersections and Crossings.

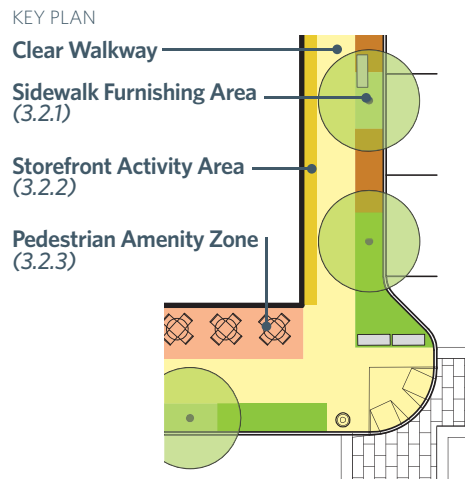


Figure 9. Key Plan of Sidewalk Zones
The above image depicts the intent of the design guidelines and is for illustrative purposes only

3.2.2 Storefront Activity Area

The Storefront Activity Area is a 2'-3' strip along the streetwall that should be used as the "front yard" of the store premises to advertise and contribute color and vitality to the street through the use of merchandise carts, easels, displays, flower pots and planters. See Figure 9.

- Use this area to display items associated with the merchandise sold inside the store.

Recesses

- Consider incorporating shallow recesses into the streetwall, as allowed per the SZD, to provide space for entrances, and places for seating, and retail displays.
- Limit recesses to within 8' of the streetwall.

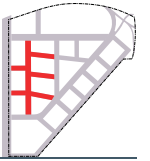


Retail establishments can have "front yards" with displays or contain recesses to create a public space

3.2.3 Pedestrian Amenity Zone

The Pedestrian Amenity Zone is an additional 10' zone located *only* along the Retail Streets that intersect with 126th Street. This zone is required per the SZD and intended to allow ample space for a grade change transition and connection to the upper and lower levels of 126th Street. See Figure 9.

- In addition to the grade change, consider using this widened sidewalk area for additional amenities such as fixed or cafe seating, plantings, retail displays, streetscape furniture, sculpture, etc.
- Maintain a clear walkway connecting to the primary clear walkway of the sidewalk.
- Minimize split-level sidewalks, as necessary to connect to the upper and lower levels of 126th Street.
- Design transition treatments along the split-level sidewalk to include generous open stair systems with informal seating areas. See also 3.1.2 Transition Zone: Graceful Grade Change.
- Consider distinctive pavement to demarcate the area.
- Conceal service and garage entries that are located adjacent to seating areas or other pedestrian areas with landscape buffers.



3.2.4 Pedestrian-Oriented Intersections and Crossings

Retail Streets should be designed to have pedestrian friendly intersections and crossings to support easy movement along and across a two-sided retail corridor. See Figure 11.

- In order to provide more generous pedestrian areas, consider expanding the Sidewalk Furnishing Area into the on-street parking lane at intersections and crossings.
- Shaded social seating, trees and plantings, public art, bike racks, orientation map kiosks, newsstands, trash receptacles, lighting, and other streetscape amenities are appropriate in these expanded areas.
- Pedestrian crosswalks are encouraged for every 200' along the Primary Retail Street. Mid-block crossings can be provided to meet this goal.
- Consider use of a specialized NYCDOT-approved paving to call attention to pedestrian crossings for motorists.



Expanded sidewalk areas can include seating, shade trees, and other amenities

3.2.5 Planted Median

The 8'-10' wide planted median is an opportunity for additional landscape as well as sustainable stormwater conveyance on the Primary Retail Street.

- Consider the use of a low-impact drainage system made up of interconnected planting medians that connect to the storm drain.
- Provide circulation across the median at all intersections and mid-block crossings.
- Mark circulation across the median by at least 2 specialty NYCDPR-approved trees per crossing in the median.
- For additional guidelines specific to the median, see Chapter 2 Design Quality Standards: Public Realm.



Figure 10. Illustrative view of the Primary Retail Street with planted median
The above image depicts the intent of the design guidelines and is for illustrative purposes only



Figure 11. Diagram of the Primary Retail Street
The above image depicts the intent of the design guidelines and is for illustrative purposes only

3.2 RETAIL STREETS **Architectural Guidelines**

For Guidelines regarding issues common to all building facades, see Chapter 2 Design Quality Standards: Architecture.

3.2.6 Active Retail Lower Base

The character of the lower base is critical to defining an active retail environment. Create a continuous streetwall with retail storefronts of varying sizes. Corners, due to their higher visibility, should contain the larger establishments. Provide recesses for busy entrances, seating or retail displays and to create variety. Provide direct street access to all establishments, including second floor retail.



Soho, New York

3.2.7 Upper Base Creates a Consistent Streetwall

Locate residential and commercial uses in the upper base to contribute to a mixed-use environment and provide 24/7 "eyes on the street". Design facades to have high transparency, vertical articulation and setbacks that humanize the scale of the buildings as perceived by the street-level pedestrian. Use recessed balconies and large windows to reinforce residential character above retail shops.



Portland, Oregon

3.2.8 Mid-rise Articulated from Upper Base

Setback the mid-rise portions of buildings from the upper base. Similar to the lower base, design these facades to include balconies and other architectural articulation that breaks down the scale and horizontality of the buildings. Consider use of the "dormer rule" to allow projections into the setback above 85', creating a more dynamic mid-rise roofline.



Toronto, Canada

3.2.9 Towers Setback from Mid-rise

Design towers along the Retail Streets to form an urban backdrop but not dominate the streetscape environment. As required per the SZD, all towers must setback 10'-15' from the base.



Battery Park City, New York

Anchor Blocks

The Anchor Blocks form the first impression of Willets Point and the pedestrian gateways to the site. Wide footprints accommodate large-format retail uses and mixed-use towers that anchor both ends of the Primary Retail Street. The Southern Anchor Block includes a sequence of pedestrian plazas leading from Roosevelt Avenue into the interior of Willets Point.

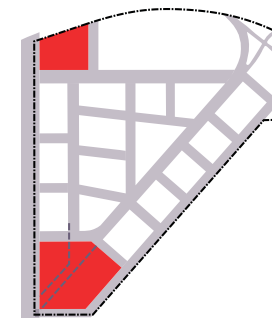


Figure 1. Diagram of the Anchor Blocks
The above image depicts the intent of the design guidelines and is for illustrative purposes only

3.3 ANCHOR BLOCKS Overview

OVERVIEW

The Northern and Southern Anchor Blocks mark two of the three entry points into Willets Point at the north and south ends of 126th Street respectively (the third being the Van Wyck Expressway at the northeast corner of the site). The Anchor blocks serve several key functions in the site plan.

As the most visible buildings from major roads, highways, and rail lines, the Anchor Blocks are visual symbols of Willets Point and will mark the gateways to the District on the skyline. Towers on these sites are prime locations for iconic architecture that showcases the sustainability features of the entire District. Setbacks along Northern Boulevard and Roosevelt Avenue provide for greenery, light and air between the elevated highways and rail lines and these new buildings.

With prime subway access at the Southern Anchor Block, and adjacency to the Convention Center at the Northern Anchor Block, these blocks are opportunities for commercial and destination uses such as offices, hotels, and cinemas.

Note: The Southern Anchor Block and Willets Point Boulevard easement are shown assuming existing utilities are raised to new grade. The design intent, principles, and guidelines outlined in this chapter are relevant regardless of changes in the utility alignment. In the event that the utilities are not relocated or raised, an alternative design may be required. See pages 100-101 for guidelines regarding the Southern Anchor Block Alternate Design. Improvements within the easement to be coordinated with City agencies.

These design guidelines assume that the entire retail and entertainment zone of Area A, including 126th Street, the Retail Streets, and the Anchor Blocks function as a retail district. As part of this concept, the Anchor Blocks accommodate large-format and multi-story retail and entertainment uses which bookend smaller “in-line” stores lining the Primary Retail Street and Retail Streets. Interior atria, generally discouraged to ensure street-oriented retail, can be accommodated in the Anchor Blocks to access multi-story retail.

The Anchor Blocks also create important pedestrian gateways into the interior of the site. The Southern Anchor Block includes a series of signature public plazas that will replace a segment of Willets Point Boulevard. Although the right-of-way must remain open due to an existing utility easement, the street segment will be closed to vehicles and converted into a landscaped pedestrian space that provides access from the subway directly into the interior of Willets Point and a view corridor into the District.

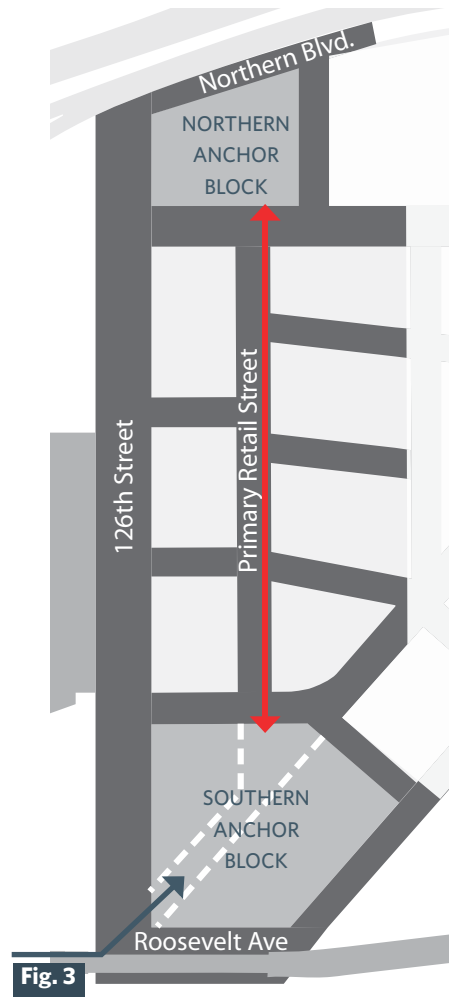


Fig. 3

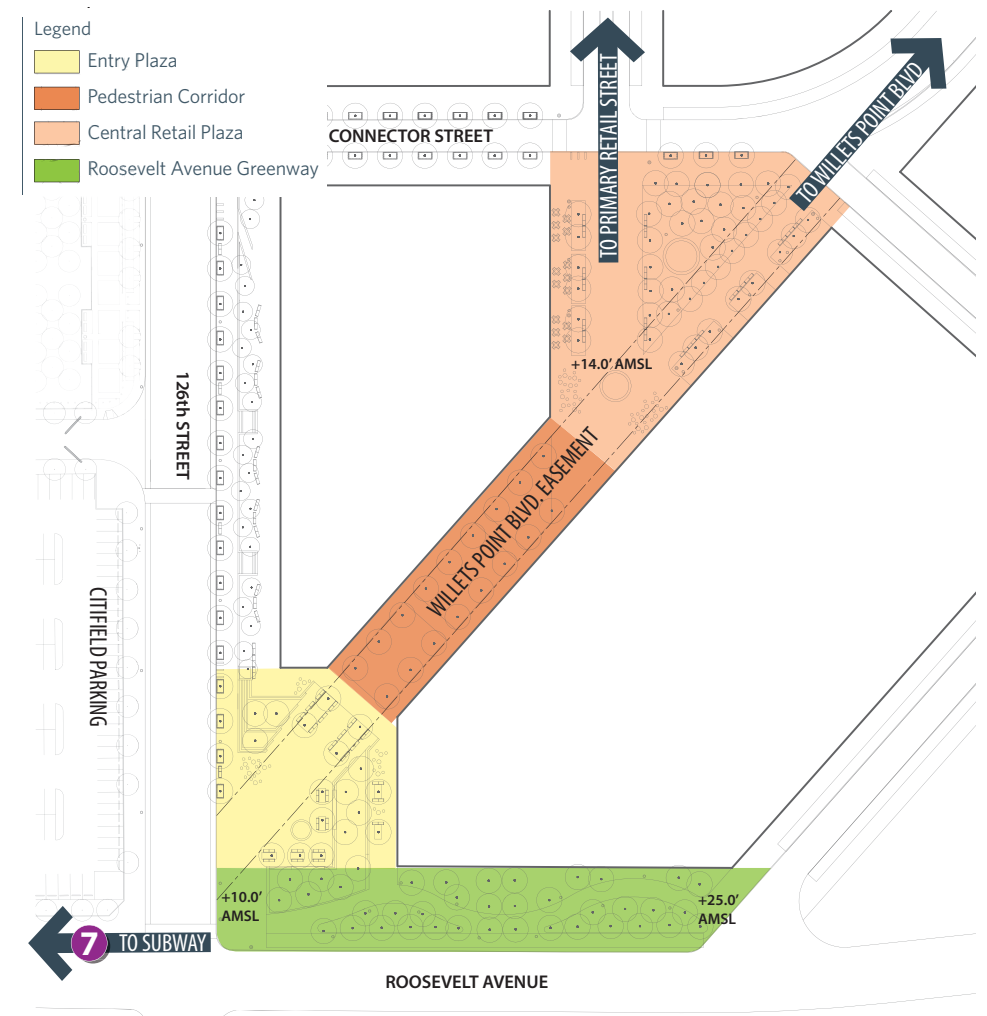
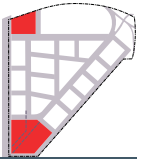


Figure 2. Key plans of the Anchor Blocks (left) and the Southern Anchor Block public open spaces (right)
 The above image depicts the intent of the design guidelines and is for illustrative purposes only.

All elevation grades in this document are in feet NGVD29 with a FEMA 100yr floodplain of 14 feet NGVD29.



PUBLIC REALM GUIDELINES

See pages 94–101 for details.

The Southern Anchor Block provides a series of pedestrian-only plazas on the existing right-of-way of Willets Point Boulevard, forming a walkable gateway into the Willets Point site.

3.3.1 Entry Plaza

Entry Plaza is 7,000 sf minimum

3.3.2 Pedestrian Corridor

Minimum 60' wide corridor at new grade level (+14 AMSL)

3.3.3 Central Retail Plaza

20,000 sf minimum at new grade level (+14 AMSL)

3.3.4 Roosevelt Avenue Greenway

Pedestrian and bicycle amenity zone

ARCHITECTURAL GUIDELINES

See pages 104–106 for details.

The Anchor Blocks form signature icons of Willets Point to the outside while framing pedestrian plazas and streetscapes on the inside.

3.3.5 Signature Towers

3.3.6 Framing the Pedestrian Gateway

3.3.7 Building Base: Perimeter Facades

3.3.8 Building Base: Interior Facades

3.3.9 Visual Termini to the Primary Retail Street

3.3.10 Daylit Atria



Figure 3. Illustrative view of the Southern Anchor Block from the corner of 126th Street and Roosevelt Avenue
The above image depicts the intent of the design guidelines and is for illustrative purposes only.

3.3 ANCHOR BLOCKS Public Realm Guidelines

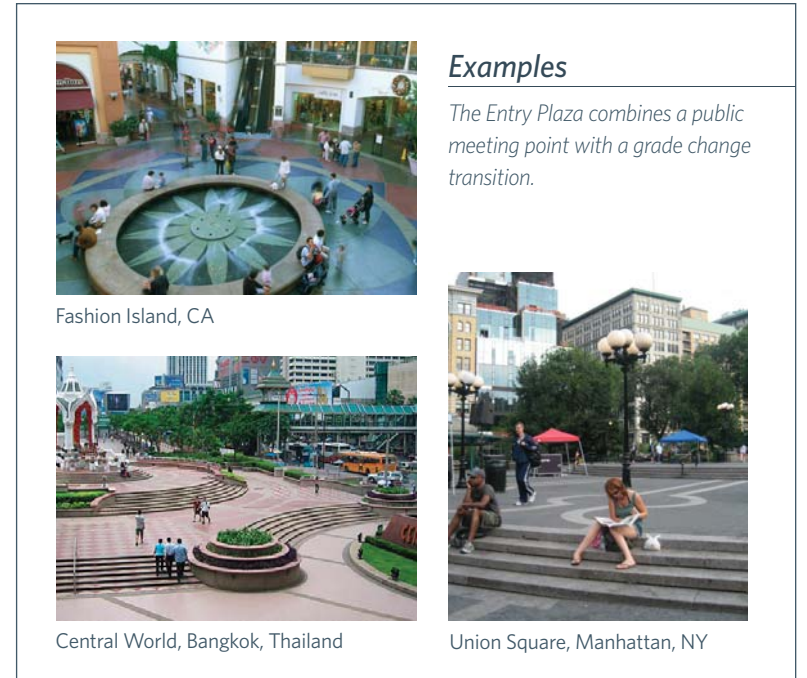
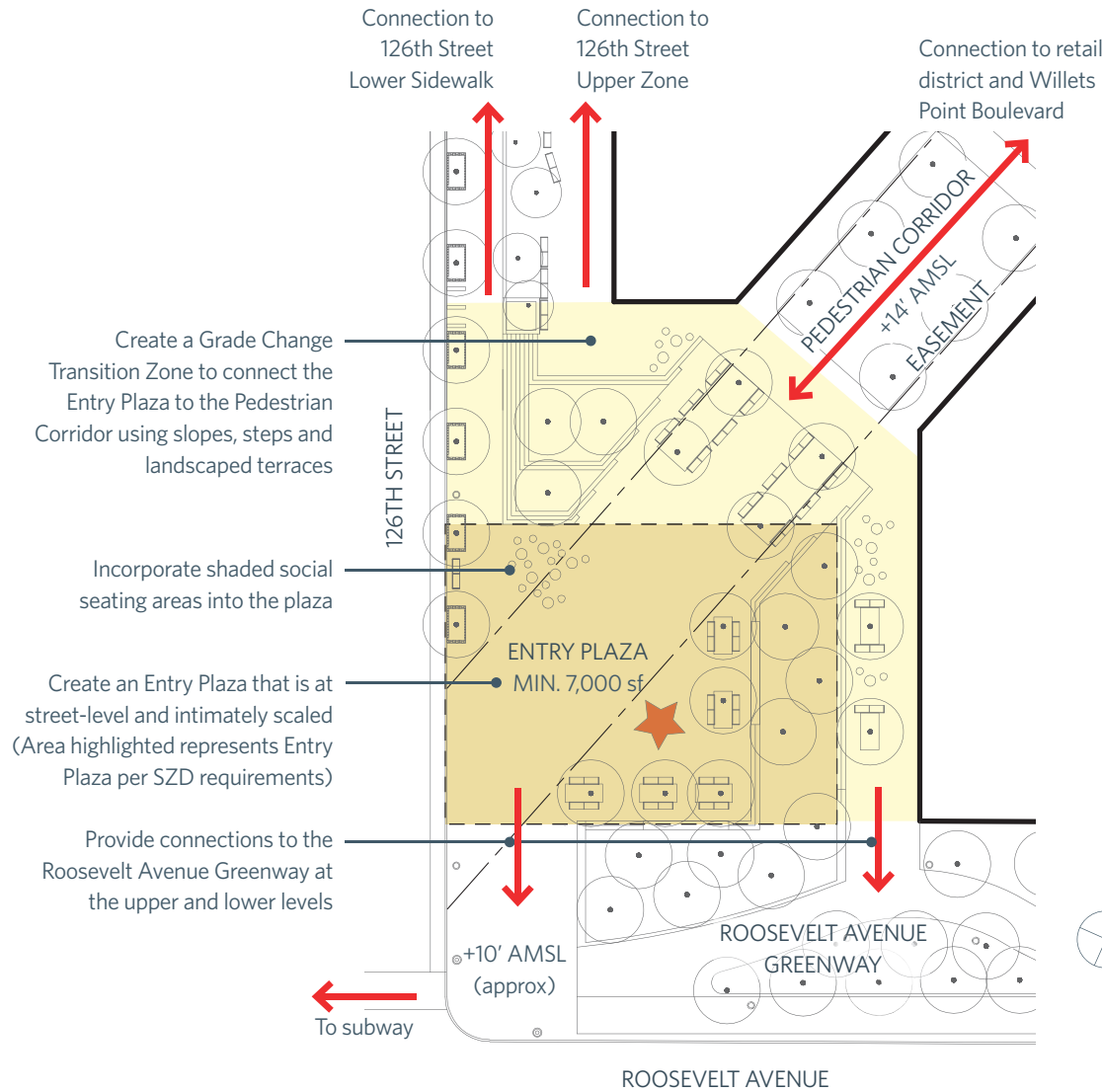
For Guidelines regarding issues common to all streetscapes and open spaces, see Chapter 2 Design Quality Standards: Public Realm.

3.3.1 Entry Plaza

The Entry Plaza is the point of entry to the District for most pedestrians, especially those coming from the subway, Corona, Flushing, or Flushing Meadows Corona Park. It should function as a welcoming and memorable meeting point for visitors and residents of the District that directs movement along 126th Street, into the retail district, and down Willets Point Boulevard. The Entry Plaza must be a minimum of 7,000 square feet per the SZD, may have more hardscape than other adjacent plazas to accommodate pedestrian flows, and may be animated by special features such as a fountain. See Figures 4 and 5.

A Grade Change Transition Zone between the existing grade (approx. +10' AMSL) and the new grade (+14' AMSL), might be provided to connect the Upper Zone of the 126th Street Linear Plaza, Entry Plaza and Pedestrian Corridor. This grade change should be designed as a terraced plaza with landscaping and shaded seating areas overlooking the Entry Plaza.

Note: For zoning purposes, the Entry Plaza and Grade Change Transition Zone shall be considered as separate spaces. All Publicly Accessible Open Space Requirements per SZD are applicable.

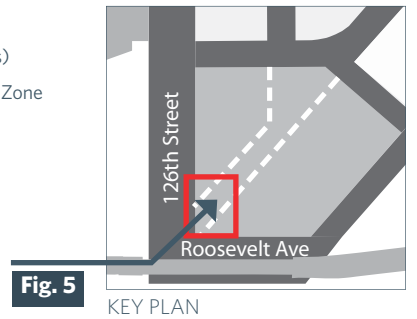


- Legend
- Entry Plaza (per SZD requirements)
 - Potential Grade Change Transition Zone
 - Water Feature or Public Art



Figure 4. Illustrative plan diagram of the Entry Plaza and potential Grade Change Transition Zone

The above image depicts the intent of the design guidelines and is for illustrative purposes only.



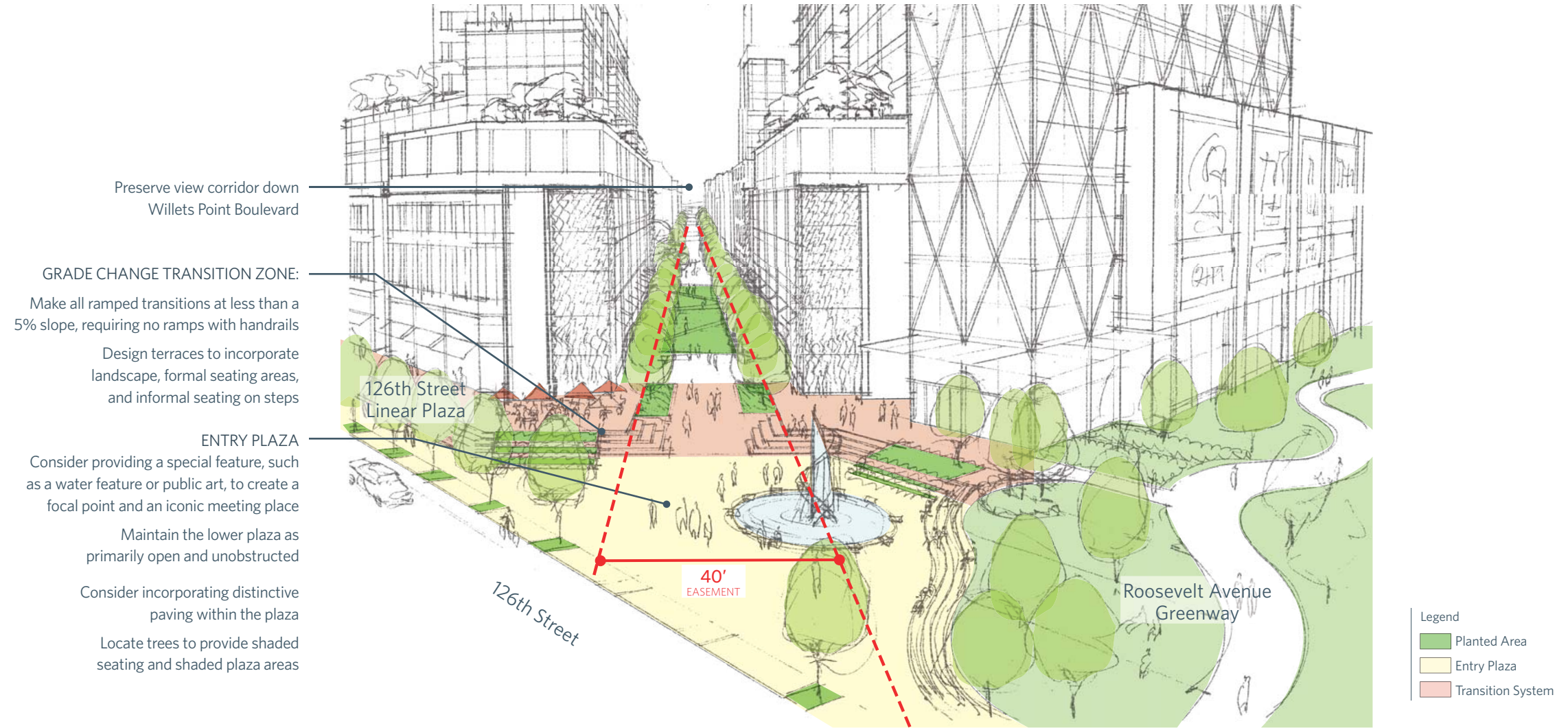
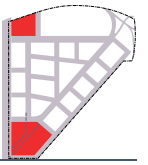


Figure 5. Illustrative view of the Entry Plaza connecting to the Roosevelt Avenue Greenway, Pedestrian Corridor, and 126th Street Linear Plaza
The above image depicts the intent of the design guidelines and is for illustrative purposes only.

3.3 ANCHOR BLOCKS Public Realm Guidelines

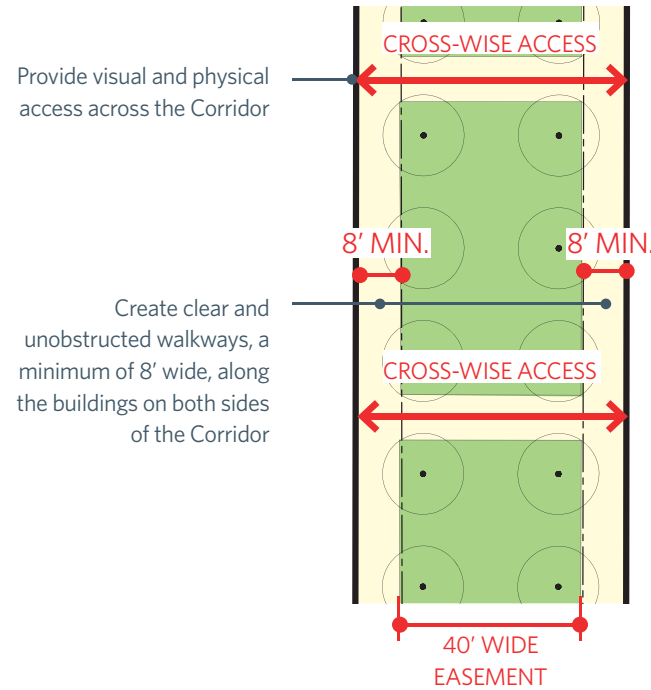
For Guidelines regarding issues common to all streetscapes and open spaces, see Chapter 2 Design Quality Standards: Public Realm.

3.3.2 Pedestrian Corridor

The Pedestrian Corridor is a minimum 60' wide linear space that should be designed as a landscaped pedestrian way with retail storefronts and building entrances on both sides with easy access across the corridor as well as along it.

This Pedestrian Corridor has two walkways, one on either side facing storefronts with space for merchandise displays and seating. See 3.2.2 Storefront Activity Area for further guidelines. The central zone, framed by trees, is a green corridor with lawns and gardens, shaded seating, and cross-wise pathways. See Figures 6 and 7.

Note: all Publicly Accessible Open Space Requirements per SZD are applicable.



- Legend
- Planted Areas
- Hardscape Areas



Figure 6. Illustrative plan diagram of the Pedestrian Corridor

The above image depicts the intent of the design guidelines and is for illustrative purposes only.

Examples

The Pedestrian Corridor should be a landscaped connective corridor with retail shops and displays framing both sides.



Rockefeller Center, Manhattan, NY



Lincoln Road, Miami, FL



Pearl Street, Boulder, CO

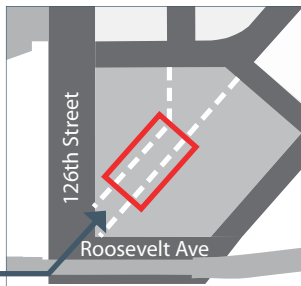


Fig. 7 KEY PLAN

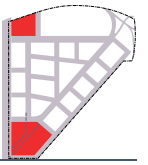


Figure 7. Illustrative view of the Pedestrian Corridor
The above image depicts the intent of the design guidelines and is for illustrative purposes only.

3.3 ANCHOR BLOCKS Public Realm Guidelines

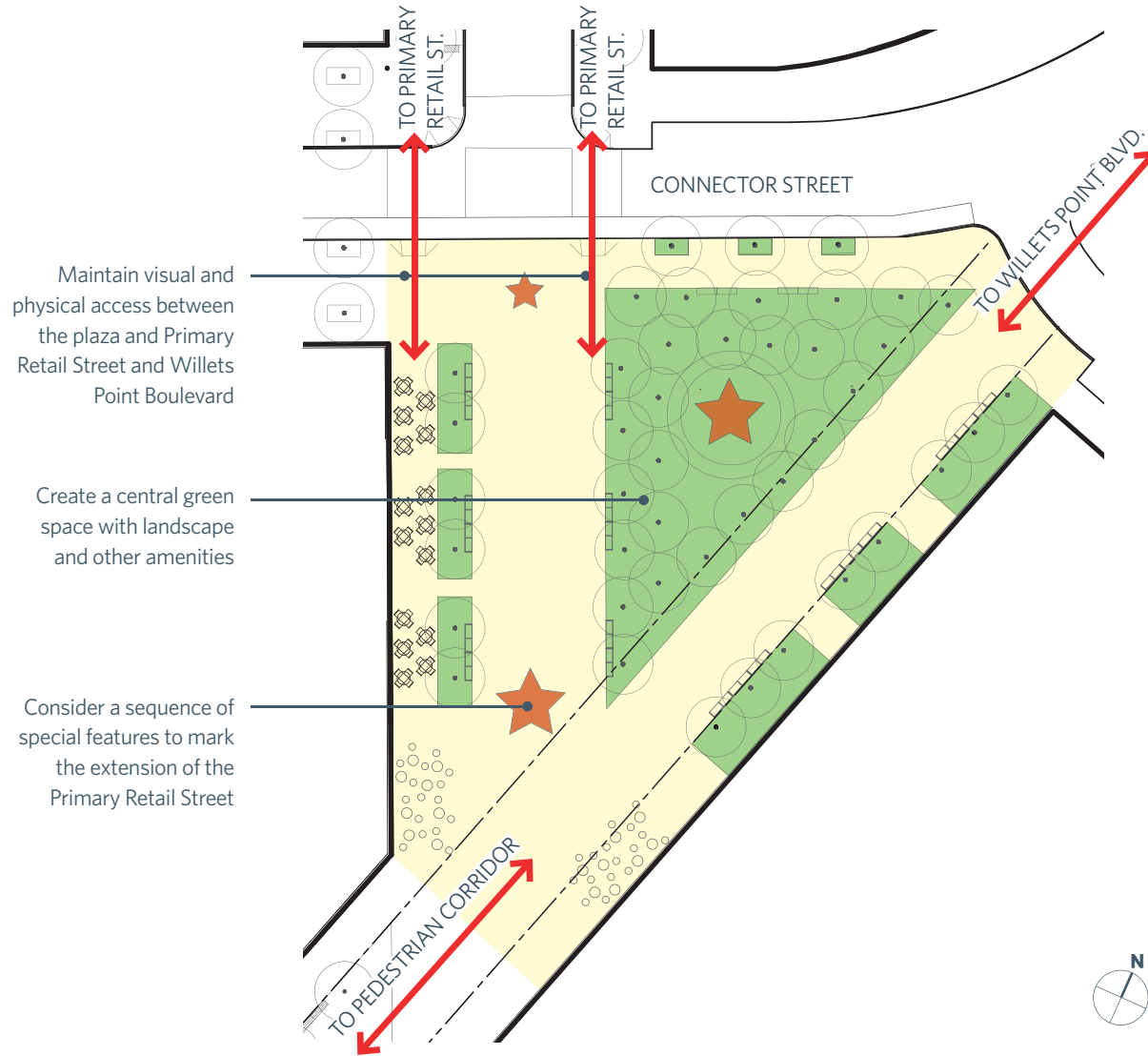
For Guidelines regarding issues common to all streetscapes and open spaces, see Chapter 2 Design Quality Standards: Public Realm.

3.3.3 Central Retail Plaza

The Central Retail Plaza is a signature open space that balances multiple functions. It is a year-round gathering space and rest area for retail patrons and visitors, and also a plaza serving residents of Willets Point. As such, it should appeal to multiple age groups with an emphasis on children, and balance occasional events and activities with passive enjoyment that is always open to the general public.

Extensive greenery and shade can be combined with features such as outdoor seating and dining areas, water features, tot-lots or play areas, public art, and small gathering spaces or amphitheatres. See Figures 8 and 9.

Note: all Publicly Accessible Open Space Requirements per SZD are applicable.



Maintain visual and physical access between the plaza and Primary Retail Street and Willets Point Boulevard

Create a central green space with landscape and other amenities

Consider a sequence of special features to mark the extension of the Primary Retail Street

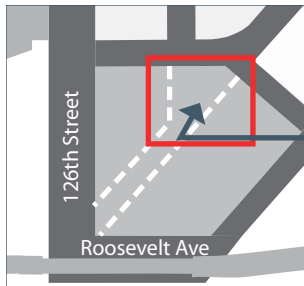

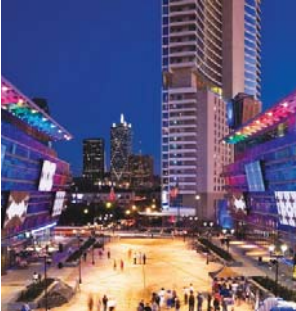


Fig. 9


KEY PLAN



Jamison Park, Oregon



Victory Park, Texas



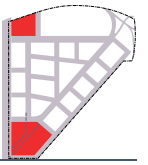
Millennium Park, Chicago

Examples

The Central Retail Plaza should serve both residents and visitors with family-oriented amenities, and space for performances and festivals.

- Legend
- Central Green Space
 - Hardscape Area
 - Special Feature (water feature, public art, etc.)

Figure 8. Illustrative plan diagram of the Central Retail Plaza
The above image depicts the intent of the design guidelines and is for illustrative purposes only.



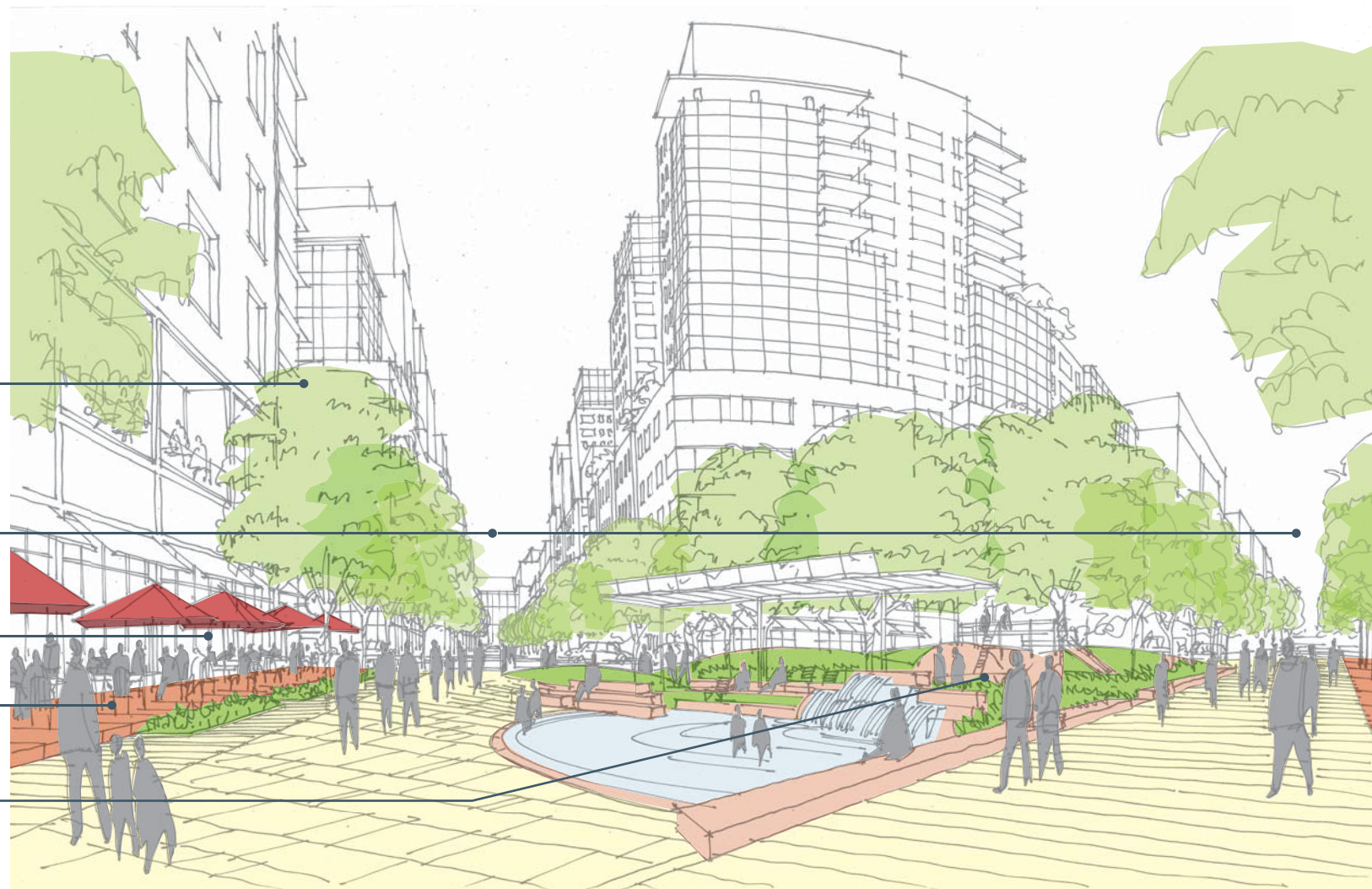
Although the SZD requires approximately 20 trees, additional trees are recommended in order to create a park-like environment and shaded seating areas adjacent to the retail

Maintain view corridors to Primary Retail Street and Willets Point Boulevard

Consider cafe-style dining in the plaza

Throughout the plaza and especially along the storefront edges, consider incorporating distinctive paving

This central green space can include gardens, shaded seating, and features, such as play areas, lawns, fountain, or gathering spaces



- Legend
- Planted Area
 - Plaza
 - Central Green Space
 - Storefront Areas

Figure 9. Illustrative view of the Central Retail Plaza
The above image depicts the intent of the design guidelines and is for illustrative purposes only.

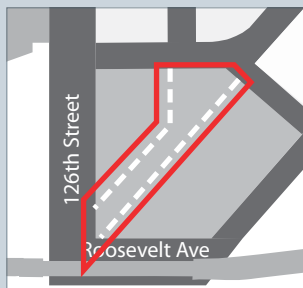
3.3 ANCHOR BLOCKS Public Realm Guidelines

For Guidelines regarding issues common to all streetscapes and open spaces, see Chapter 2 Design Quality Standards: Public Realm.

Alternate Design Southern Anchor Block

In the event that existing utilities below Willets Point Boulevard are not raised or relocated, the area of the easement must remain at existing grade per NYCDEP requirements. Improvements within the easement to be coordinated with City agencies. The Alternate Design shown here provides for a two-level series of public spaces on the Southern Anchor Block.

Consistent with the designs on the preceding pages, the public spaces are intended to provide a welcoming gateway Entry Plaza at Roosevelt Avenue and 126th Street, an active Pedestrian Corridor that provides open space for gathering as well as access to the retail shops and commercial establishments, and a Central Retail Plaza that is a public destination and link to the Primary Retail Street and residential neighborhood beyond.



KEY PLAN

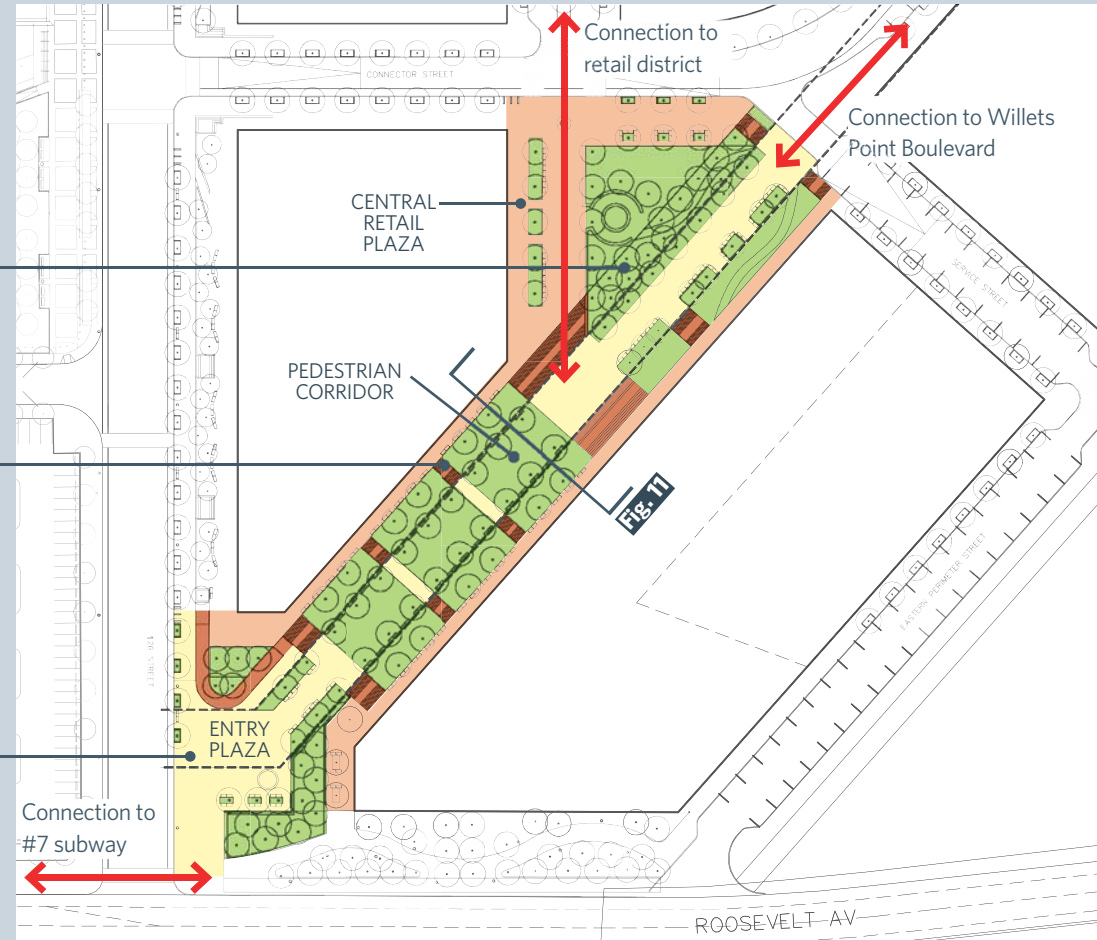
Integrate planted areas in the Transition Zone with planted areas at the Upper and Lower levels

Provide frequent access between the Upper Level and Lower Level with stairs and ramps

Align stairs and ramps with storefront entries where possible

Facilitate cross-wise circulation between the buildings

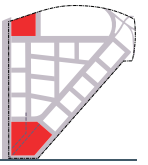
Facilitate circulation through the block by providing clear access from end to end



- Legend
- Lower Level at Existing Grade
 - Transition Zone
 - Upper Level at New Grade
 - Planted Areas



Figure 10. Illustrative diagram of the Alternate Design for the Southern Anchor Block open spaces
The above image depicts the intent of the design guidelines and is for illustrative purposes only.



Design the Upper Level to allow ample space for circulation along it and access into the storefronts

Use the Transition Zone to provide sloping and/or terraced planted areas

Avoid plinth walls over 18" in height

Provide good visual access between the Lower Level and Upper Levels

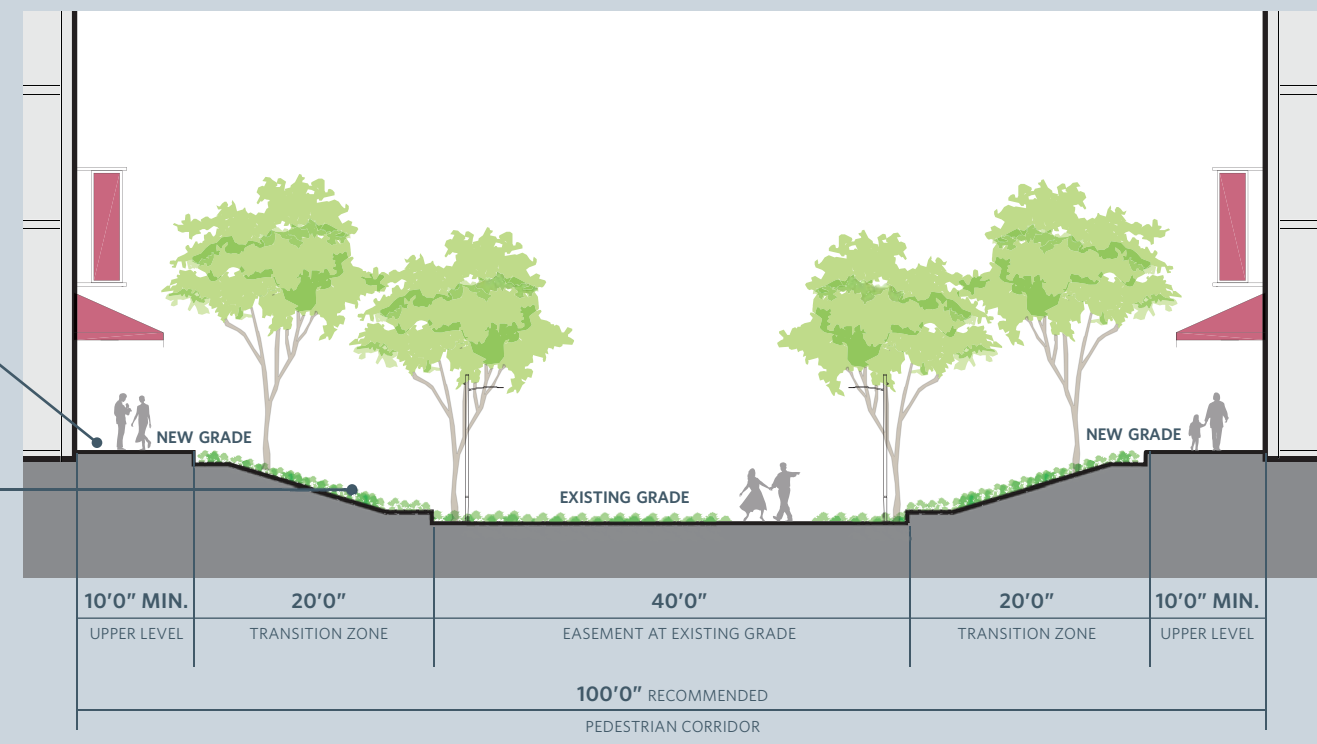


Figure 11. Illustrative sectional diagram of the Alternate Design for the Pedestrian Corridor
The above image depicts the intent of the design guidelines and is for illustrative purposes only.

3.3 ANCHOR BLOCKS Public Realm Guidelines

For Guidelines regarding issues common to all streetscapes and open spaces, see Chapter 2 Design Quality Standards: Public Realm.

3.3.4 Roosevelt Avenue Greenway

The design of this greenway should form an edge to the District that is characterized by greenery and landscaping and provides a gracious setback of new buildings from Roosevelt Avenue and the elevated subway structure.

The SZD requires a Pedestrian Amenity Area between 20' and 45' and a 5' bicycle lane, however these guidelines recommend a 10' two-way bicycle lane. Due to a steep existing grade change, the greenway must also provide an ADA-accessible walkway from the Roosevelt Avenue Bridge sidewalk to the intersection of Roosevelt Avenue and 126th Street. See Figures 12-14.

Note: all Publicly Accessible Open Space Requirements per SZD are applicable.

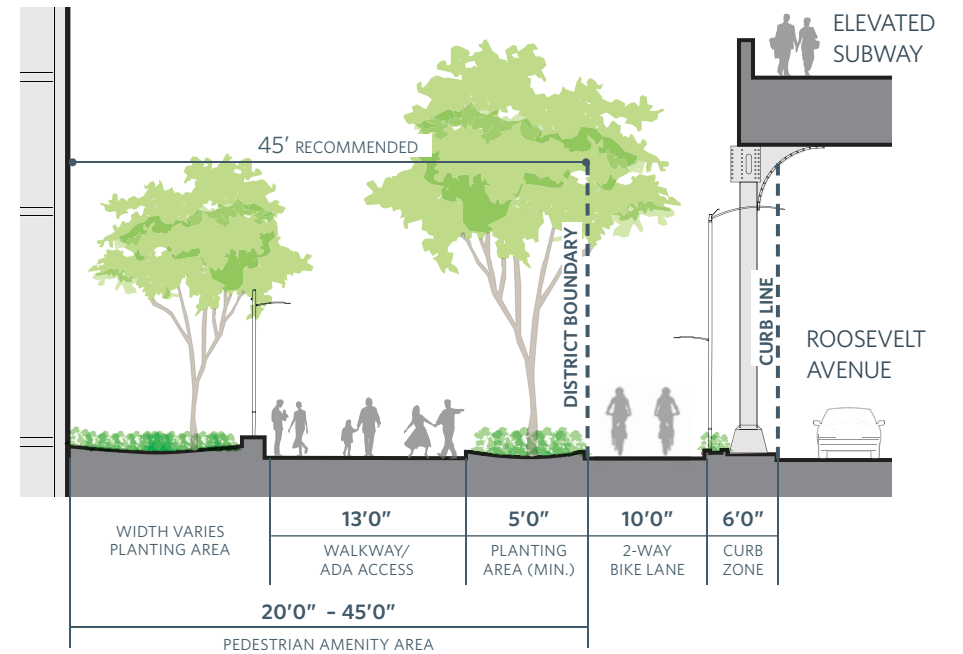
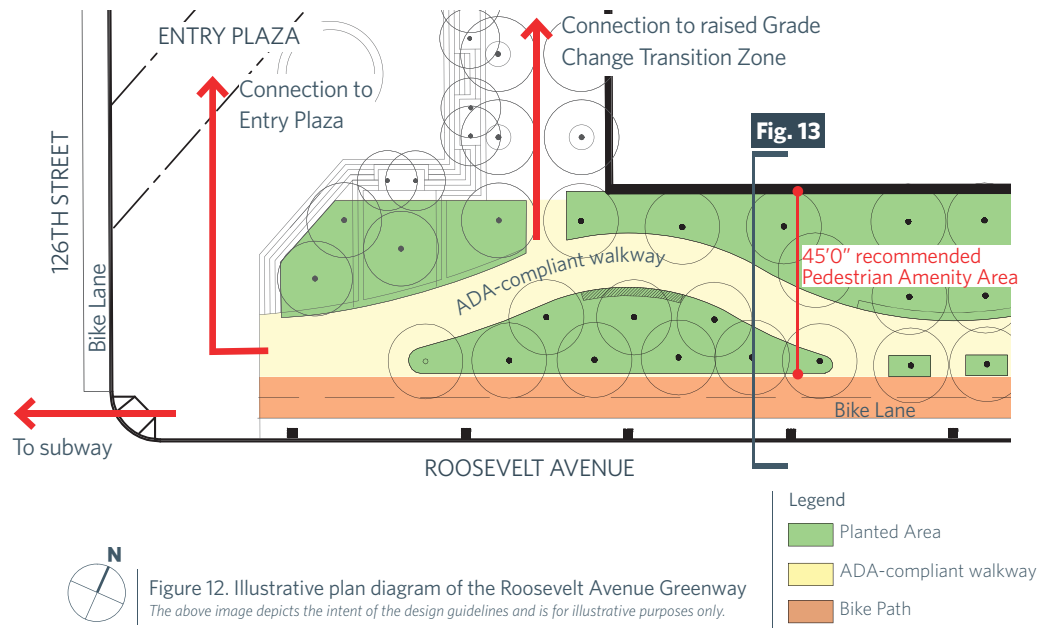
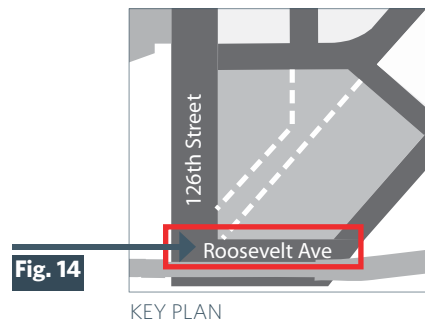
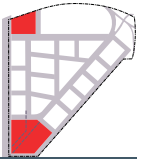


Figure 13. Illustrative sectional diagram of the Roosevelt Avenue Greenway. The above image depicts the intent of the design guidelines and is for illustrative purposes only.



Existing condition at the intersection of Roosevelt Avenue and 126th Street, showing the elevated subway structure

Although the SZD requires approximately 20 trees, additional trees are recommended in order to emphasize this green edge of the District

Provide a separated 10'-wide 2-way bike path to connect to a planned route along Roosevelt Avenue

Incorporate an ADA-accessible pedestrian walkway, at less than 5% slope, into the greenway for access between the Roosevelt Avenue Bridge sidewalk and 126th Street

Locate planting areas both along the building as well as between the pedestrian path and the roadway to act as a buffer. Although the SZD requires 50% of the Pedestrian Amenity Area to be planted, 80% is recommended

- Legend
- Planted Area
 - ADA-compliant Walkway
 - Bike Lane

Figure 14. Illustrative view of the Roosevelt Avenue Greenway
The above image depicts the intent of the design guidelines and is for illustrative purposes only.

3.3 ANCHOR BLOCKS **Architectural Guidelines**

For Guidelines regarding issues common to all building facades, see Chapter 2 Design Quality Standards: Architecture.

3.3.5 Signature Towers

The towers within the Northern and Southern Anchor Blocks will be prominent visual markers of Willets Point in the skyline, seen from great distances and from elevated highway and rail lines. These towers provide opportunities for destination uses such as offices or hotels, and iconic architecture that showcases the sustainability features of the entire District. Per the SZD, they may rise with no setbacks to provide visual gateways to the site. See Figure 15.



Figure 15. The Pedestrian Gateway framed by “green walls”, a Signature Tower and other visible sustainable design elements

3.3.6 Framing the Pedestrian Gateway

The two buildings of the Southern Anchor Block will frame either side of the Pedestrian Corridor and should be coordinated to create a dramatic architectural gateway, drawing the eye into the interior and maintaining an open view corridor down Willets Point Boulevard beyond. Maintain the view corridor open to the sky. Bridges, archways or obstructions are not permitted to extend over the Pedestrian Corridor (per zoning). See Figure 15.



Central City Tower, British Columbia



Solar panel and LED media wall, China

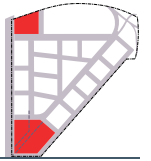


Consorcio Tower green wall, Chile



Entrance canopy

The gateways to the District should be prominently announced by the architectural treatment of buildings and by showcasing sustainable features



Building Base Guidelines

The Northern and Southern Anchor Blocks are envisioned to accommodate multi-level retail and entertainment uses. As a result, the building base including both lower base and upper base may consist of retail uses. With very different conditions on the perimeter of the site as in the interior, the building bases on Anchor Blocks have special design guidelines. See Figures 16 and 17.

If the upper base is not retail, then the design guidelines for other parts of Area A will apply.

3.3.7 Building Base: Perimeter Facades

With high visibility from roads and elevated highways and rail lines, the building base at the site perimeter (facing Northern Boulevard and Roosevelt Avenue) is an opportunity to draw attention to uses within the buildings and the site.

The lower base should provide safety and transparency for pedestrians on Northern Boulevard and especially Roosevelt Avenue; with a recommended minimum of 50% transparency similar to other areas.

The upper base, if enclosing uses such as retail or cinemas, may require areas of blank wall on the site perimeter. If blank walls are required, they are encouraged to be mitigated with architectural articulation, material and color variety, green walls, or public art. As high visibility creates a key opportunity for signage, any such signage should be integrated with the architectural design of the facade. See Figure 15.

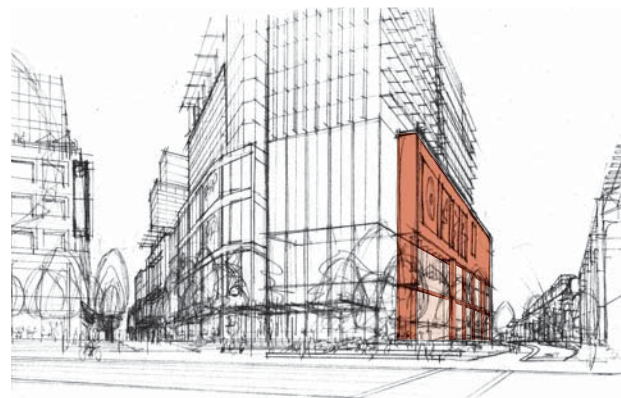


Figure 16. Perimeter Facade



Blank walls of upper floors with signage and architectural treatment appropriate for a Perimeter Facade
Siam Paragon Shopping Centre, Singapore



Highly transparent facade appropriate for an Internal Facade
Union Square, Manhattan

3.3.8 Building Base: Interior Facades

As active pedestrian retail frontages, the lower base is encouraged to follow all the same design guidelines as for the lower base on the Retail Streets and contain frequent retail and building entrances from pedestrian plazas and sidewalks. See Figure 16.

The upper base facing the interiors of the Anchor Blocks may enclose retail and entertainment uses. In this case, higher transparency is encouraged on interior facades than on perimeter facades. Light, open facades are preferable to blank walls above the pedestrian plazas and streets of the site interior. Signage at the upper base is encouraged to be integrated with the architectural design or to be behind the glass.

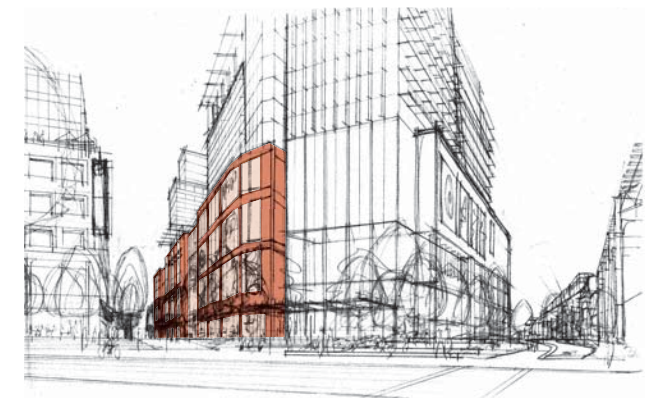


Figure 17. Internal Facade

3.3 ANCHOR BLOCKS Architectural Guidelines

For Guidelines regarding issues common to all building facades, see Chapter 2 Design Quality Standards: Architecture.

3.3.9 Visual Termini to the Primary Retail Street

The Northern and Southern Anchor Blocks should visually terminate the axis of the Primary Retail Street. This strategy may be achieved with architectural features, facade artwork, or multi-story glass-enclosed atria. See Figure 18.



Architectural feature on the facade

3.3.10 Daylit Atria

Anchor Blocks should have a strong relationship to the streets and public spaces on which they front. If provided, interior atria or gallerias should open directly onto, and act as public extensions of, outdoor plazas or sidewalks on at least one side.

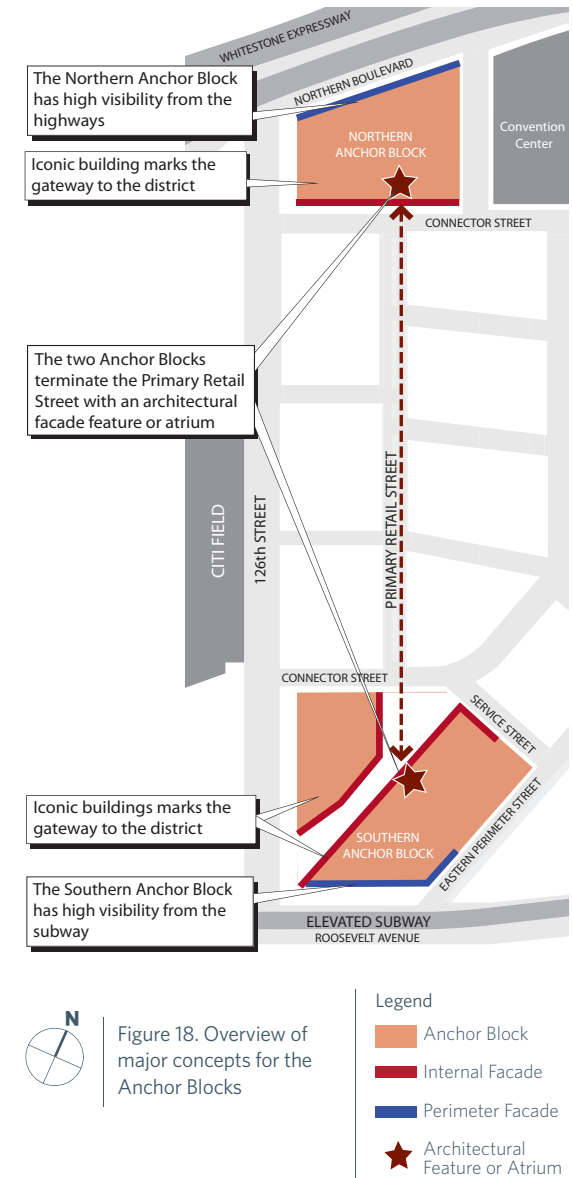
- Atria should be open to the public during regular business hours.
- Consider marking atria with large marquees or signage.
- Design atria as glass enclosed and/or skylit with abundant natural light.
- Atria may have internal circulation to connect multiple floors of retail and commercial establishments, but should also have direct access to the sidewalk or public open space.
- Outdoor public plazas as described in this chapter and required per the SZD may not be covered or enclosed.



Saar Galerie, Germany



Design atria or gallerias to be glass enclosed, filled with natural light and directly open onto an outdoor public space or sidewalk



Connector Streets

The Connector Streets are central avenues through the residential neighborhood that begin at the district gateways. Willets Point Boulevard is envisioned to link all major open spaces and be a green promenade and principal corridor for site systems and stormwater strategies

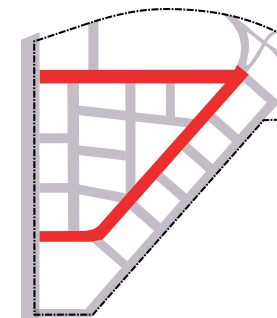


Figure 1. Diagram of the Connector Streets

The image at left depicts the intent of the design guidelines and is for illustrative purposes only

3.4 CONNECTOR STREETS Overview

FROM GATEWAY TO NEIGHBORHOOD

The Connector Streets are the core district links between Areas A and B, as well as the link to the regional highway system. Spanning the mixed-use and residential areas, the streets contain several components including: Gateways, Retail Blocks, Transition Blocks, and Residential Blocks that should be a continuous and coordinated streetscape system.

These streets are more monumental in scale than other internal streets. As district boulevards, they are envisioned as pedestrian-oriented with major building entrances. Per the SZD, at least one of these streets will border the Neighborhood Park (Willets Point Boulevard is preferred).

Comprising two existing streets (Willets Point Boulevard and 34th Avenue), these streets are assumed to remain in their current alignments but if alignments are changed, these guidelines still pertain.

34TH AVENUE

The connection between Area A and the residential neighborhood and Convention Center in Area B. The existing street is enlarged to 75' (only 60' is currently mapped) per the SZD in order to bring the street up to the standards of the District.

WILLETS POINT BOULEVARD

The main district link that connects all major public open spaces and is the conceptual extension of the Southern Anchor Block Plaza. Its existing right-of-way is 80', making the street an extra 5' wider than the recommended 34th Avenue width.

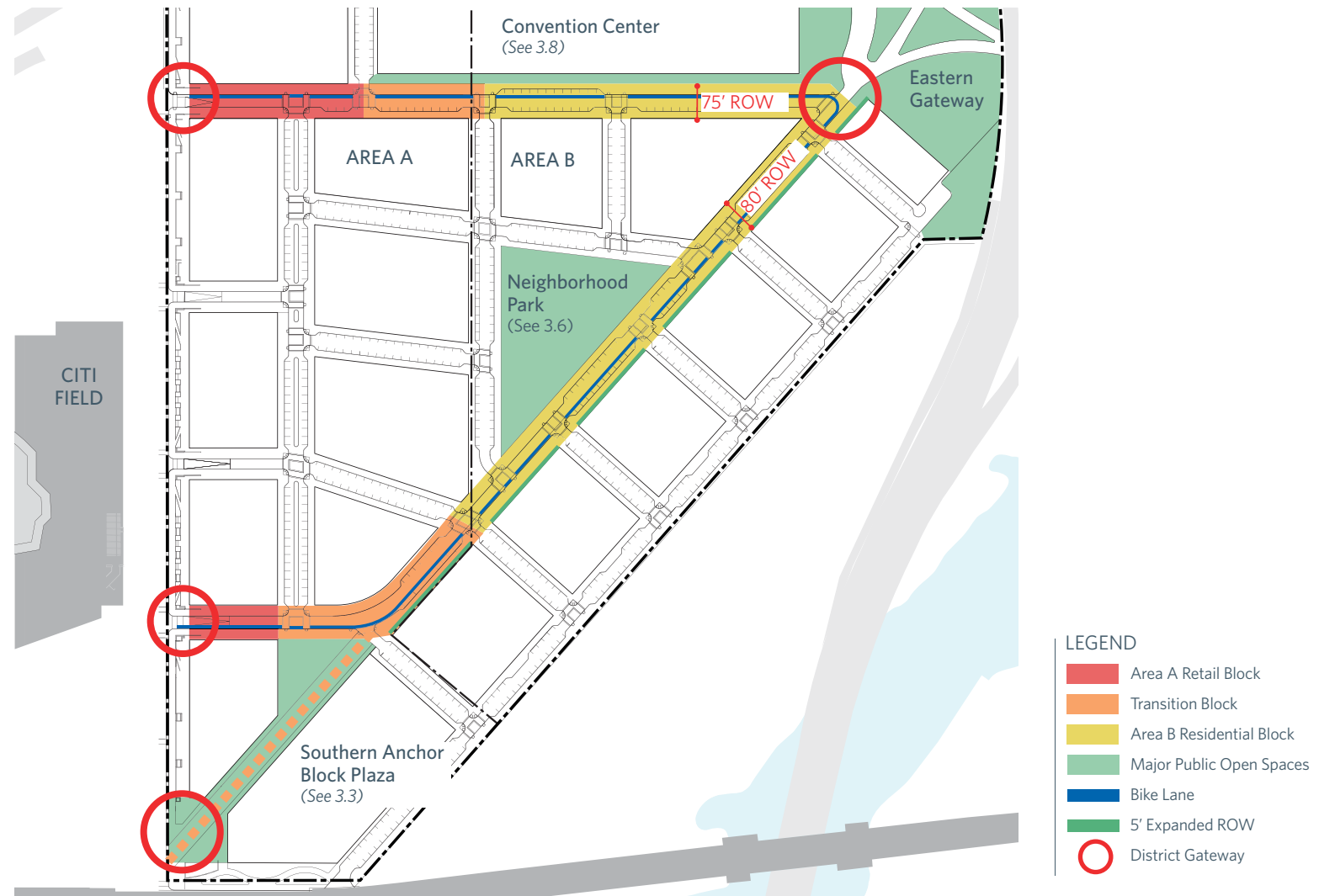
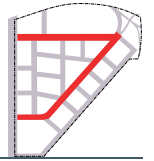


Figure 2. Diagram of the Connector Streets

The above image depicts the intent of the design guidelines and is for illustrative purposes only



PUBLIC REALM GUIDELINES

See pages 112–113 for details.

The Connector Streets provide active pedestrian-friendly spaces for retail and residential areas, connecting the district.

- 3.4.1** District Avenues
- 3.4.2** Streetscape Continuity
- 3.4.3** Pedestrian-oriented Retail and Transition Blocks
- 3.4.4** Green Residential Blocks
- 3.4.5** District Entrance: Eastern Gateway

ARCHITECTURAL GUIDELINES

See page 114 for details.

The buildings of the Connector Streets frame the major gateways and contribute to an animated district with a varying skyline

- 3.4.6** Welcoming Gateways
- 3.4.7** Active Retail and Transition Blocks
- 3.4.8** Area B Neighborhood Corner Retail
- 3.4.9** Animated Residential Buildings



Figure 3. Illustrative view of Willets Point Boulevard

The above image depicts the intent of the design guidelines and is for illustrative purposes only

3.4 CONNECTOR STREETS

RETAIL & TRANSITION BLOCKS

The Retail Blocks provide a gateway to the District while the Transition Blocks are the front door to the neighborhood.

Retail Blocks are intended to have larger and higher-intensity retail activities relating to 126th Street and the Anchor Blocks. See 3.1 126th Street and 3.3 Anchor Blocks. Transition Blocks are intended to have smaller scale retail activities, similar to the Primary Retail Street that will provide a transition in scale and intensity to the residential neighborhood. Both types are intended to contain mixed-use buildings with retail on the lower floors and residential above. See Figures 4 and 5.

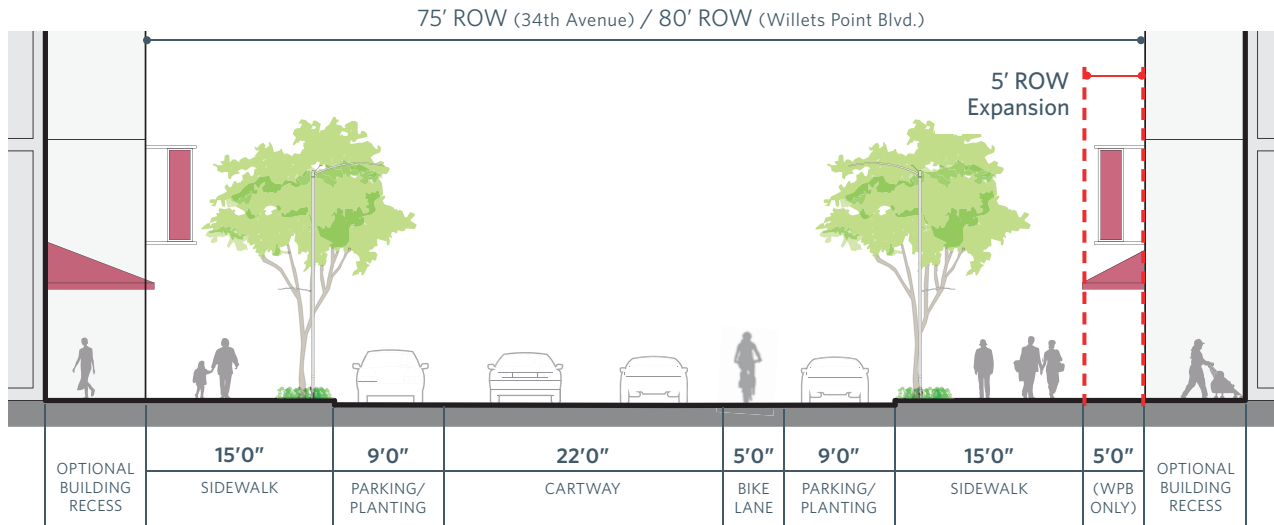
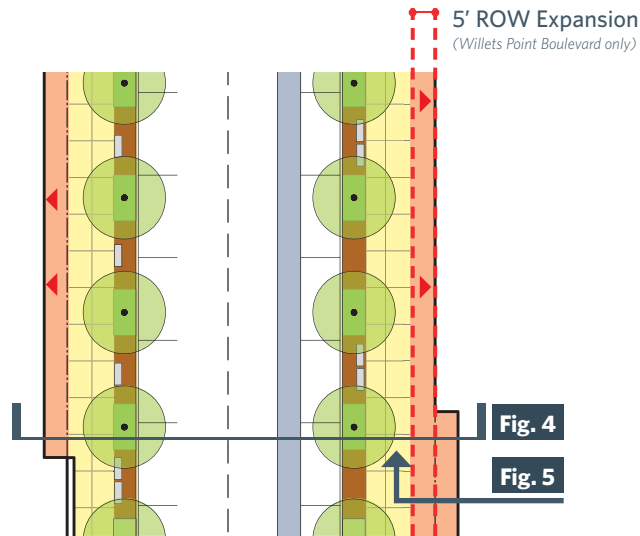


Figure 4. Typical plan and section of a transition block
The above image depicts the intent of the design guidelines and is for illustrative purposes only

NOTE: The actual layout of the street to be developed in concert with the New York City Department of Transportation.



LEGEND

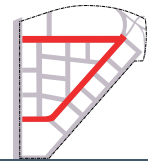
- Walkway
- Storefront Activity Area
- Planted Areas
- Bike Lane

Locate the expanded ROW at the streetwall for additional retail amenity space

5' ROW Expansion
(Willets Point Boulevard only)

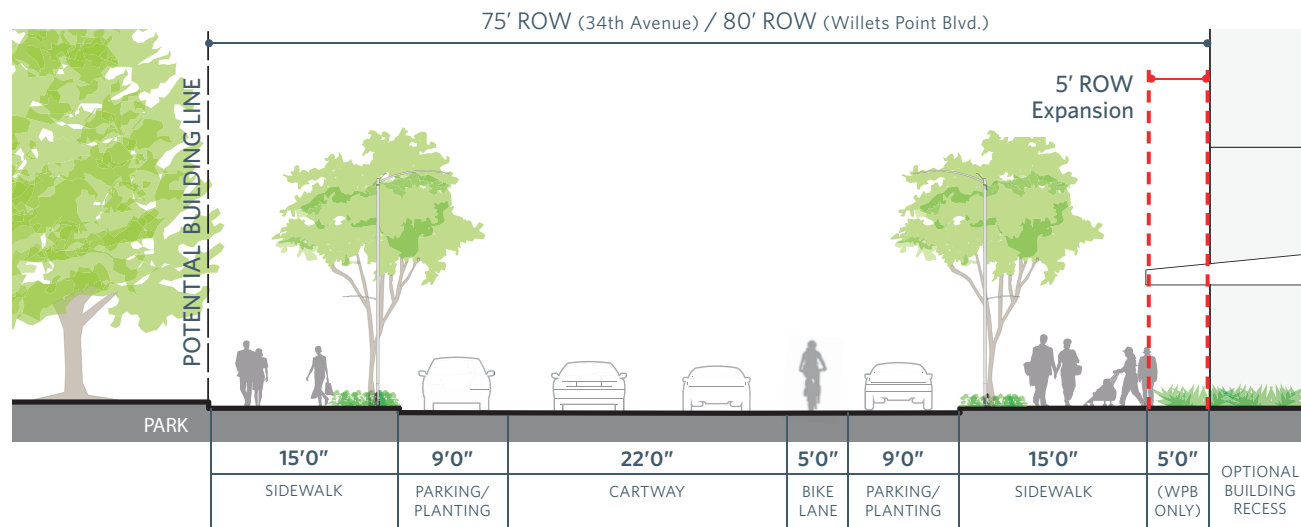
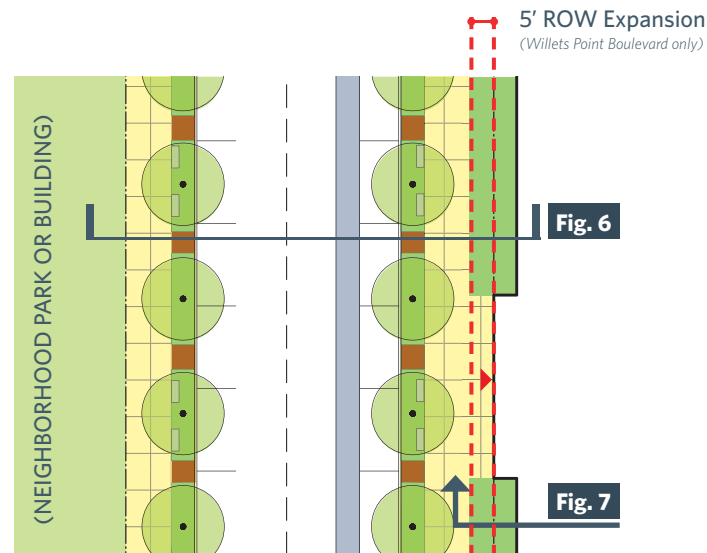
Create recess areas for seating and other retail activities

Figure 5. Illustrative sketch depicting the streetscape guidelines for a transition block
The above image depicts the intent of the design guidelines and is for illustrative purposes only



RESIDENTIAL BLOCK

The residential blocks of the Connector Street are predominantly within the Area B neighborhood. These blocks should create a friendly neighborhood streetscape with lush plantings, corner retail, and other amenities. See Figures 6 and 7.



LEGEND

- Walkway
- Streetwall Activity Area
- Planted Area
- Bike Lane

Articulate residential lobby entrances

Provide building recesses with planting strips at residential windows

Figure 6. Typical plan and section of a residential block
The above image depicts the intent of the design guidelines and is for illustrative purposes only

Figure 7. Illustrative sketch depicting the streetscape guidelines for a residential block
The above image depicts the intent of the design guidelines and is for illustrative purposes only

NOTE: The actual layout of the street to be developed in concert with the New York City Department of Transportation.

3.4 CONNECTOR STREETS Public Realm Guidelines

For Guidelines regarding issues common to all streetscapes and open spaces, see Chapter 2 Design Quality Standards: Public Realm.

3.4.1 District Avenues

Although the Connector Streets connect to the regional highways, this is not their primary purpose and they should have the character of principal neighborhood avenues.

- The scale of these avenues should feel grander than the internal retail and residential streets. Locate any large-scale streetscape furniture, such as covered bike shelters, bus stops, and wayfinding kiosks (limited to within Area A retail district) along these streets.
- Although the SZD allows for up to 3 lanes of vehicular traffic, in order to maintain a pedestrian-oriented environment, consider limiting vehicular lanes to 2.
- Provide on-street parking along all blocks except those with grade change, per the SZD.

3.4.2 Streetscape Continuity

Although the Connector Streets span two distinct subareas (Areas A and B) and may be built over time, they should feel continuous along the entire length. They should be designed and built according to the same quality standards and material selection, with coordinated street tree specimen.

3.4.3 Pedestrian-oriented Retail and Transition Blocks

Both Retail and Transition blocks are encouraged to have retail activities with outdoor displays and seating to activate the streetscape. See Figure 8.

- 1 Activate bump-out areas at corners with social seating and larger streetscape elements such as bike shelters.
- 2 Consider articulated building recesses to locate cafe seating or retail displays.
- 3 Locate the extra 5' of the Willets Point Boulevard right-of-way along the building streetwall and program this space with retail activities. Coordinate with recessed streetwall areas where appropriate.
- 4 Coordinate 10' wide curbside tree pits with the parking lane to allow easy access to parked cars.

Typical Transition Block:

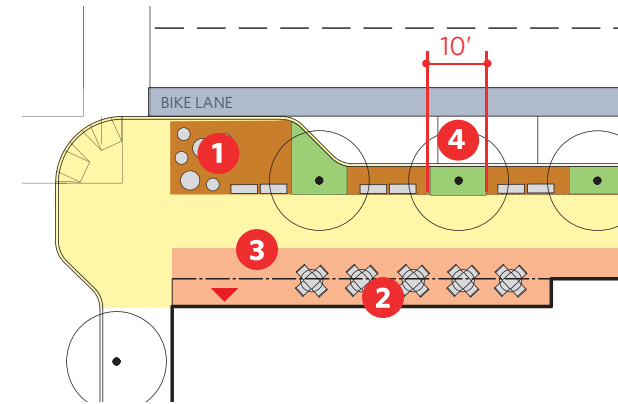
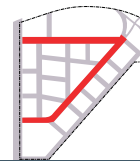


Figure 8. Diagram of a typical transition block
The above image depicts the intent of the design guidelines and is for illustrative purposes only



Seating areas at the streetwall will activate the sidewalk



3.4.4 Green Residential Blocks

In order to enhance the green character of the Connector Streets, especially Willets Point Boulevard, planted areas are encouraged to be incorporated into standard streetscape components where appropriate. See *Figure 9*.

Opportunities for greenery include:

- 1 Consider lengthening curbside tree pits to 20' long. Coordinate them with the parking lane to allow easy access to parked cars.
- 2 Activate bump-out areas at corners with planted areas and social seating.
- 3 Recess portions of the streetwall to create planted areas with decorative plantings that buffer residential windows and soften the building edge.
- 4 Locate the extra 5' of the Willets Point Boulevard right-of-way along the building streetwall to deepen planted areas and streetwall recesses where appropriate.

Typical Residential Block:

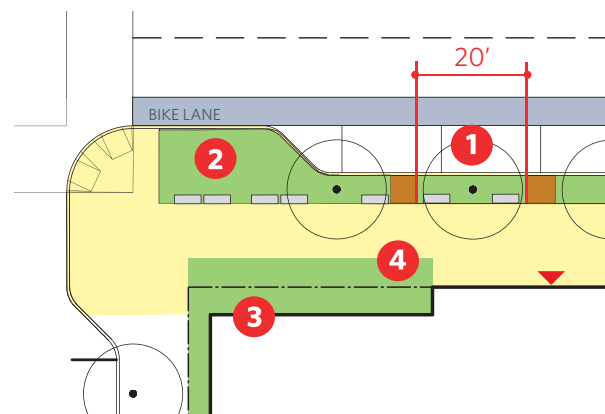


Figure 9. Diagram of a typical residential block
The above image depicts the intent of the design guidelines and is for illustrative purposes only



Larger tree pits will enhance the sense of greenery, especially when coordinated with planting at the building streetwall

3.4.5 District Entrance: Eastern Gateway

The Eastern Gateway is the major point of entry from the regional highways.

- There should be a clear and intentional entry point into the neighborhood that marks the change from a highway roadway system to a pedestrian-oriented neighborhood street grid.
- At the intersection of the Connector Streets and the highway ramps, prioritize the pedestrian experience regarding safety and circulation.
- The greenery of the Connector Streets should visually and physically connect with the landscape of the Eastern Gateway. See 3.7 *Green Edges* for guidelines regarding the landscape of this gateway.

3.4 CONNECTOR STREETS Architectural Guidelines

For Guidelines regarding issues common to all building facades, see Chapter 2 Design Quality Standards: Architecture.

3.4.6 Welcoming Gateways

AT 126TH STREET

The intersections of the Connector Streets and 126th Street are major vehicular and pedestrian gateways into Area A and the District at-large. These intersections should be marked by distinctive architecture and signage. See 3.1 *126th Street* for complete guidelines.

SOUTHERN ANCHOR BLOCK PLAZA

The Southern Anchor Block Plaza is the primary pedestrian gateway into the District. It provides a direct link to the 126th Street, Primary Retail Street and the residential neighborhood. The plaza is envisioned as a series of public spaces that graduate from more hardscape to more landscape as one walks inward. The landscape of the Connector Street and this plaza should be designed as a related sequence and considered as a continuum that eventually links with the Neighborhood Park and the Eastern Gateway. See also 3.3 *Anchor Blocks* for guidelines regarding the plaza.

EASTERN GATEWAY

The eastern buildings of the Connector Streets should frame the Eastern Gateway with distinctive architecture. Specific strategies may include taller elements (within the height limits), special corner articulation, increased glazing or other facade elements. This pair of buildings should be designed with an overall coordinated concept, massing, and materiality in order to create a strong entrance. The buildings should face the gateway with facades that have high-quality architectural variation and interest.

3.4.7 Active Retail and Transition Blocks

Retail Blocks form the gateway to the retail district and should continue the scale, intensity and storefront design of 126th Street and the Anchor Blocks. See 3.1 *126th Street* and 3.3 *Anchor Blocks*.

Transition blocks connect the western-most retail blocks to the residential fabric. These blocks are well connected to the retail district and should take advantage of this location while providing a transition in scale. The facades should be similar in scale and articulation to the Retail Streets.

- Provide ground floor retail that is of a finer-grain and complementary to the neighborhood, such as bookshops, gyms, or cafes. Avoid large or medium-box retail.
- Design storefronts as highly transparent and to include tenant signage and awnings where appropriate.
- Articulate residential lobby entrances in the streetwall facing the Connector Street and consider a streetwall recess.

3.4.8 Area B Neighborhood Corner Retail

Neighborhood retail spaces are encouraged in the Area B residential buildings to serve the resident population.

Neighborhood retail spaces should be strategically located on Connector Street corners with high visibility and easy access. Strategic locations include within proximity to the Neighborhood Park and School / Community Center.

Consider uses such as restaurants, food markets, pharmacies, doctor's offices, gyms, dry cleaners, etc.



Residential blocks should contain neighborhood retail uses at the corners

3.4.8 Animated Residential Buildings

The Connector Street buildings span the full range of allowable heights, from the tower zone to the lowest allowable height. This gradation in height should be architecturally expressed to create a varied skyline.

- Locate the primary building entrance along the Connector Street.
- Design all buildings along the Connector Street as a sequence of buildings with variation in massing, articulation, and height, considering solar access and views, especially to the Neighborhood Park.
- In Area B, locate the tallest portion of the mid-rise along the Connector Streets, not the Residential Streets.
- Consider designing facades facing the Connector Street with large windows, balconies, and architectural articulation to create an animated environment.



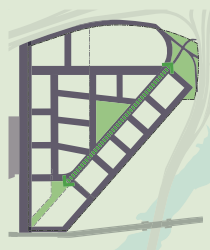
Ground floor units should have a buffer landscape at the windows for privacy and safety and to avoid protruding window accessories



ENHANCEMENT OPPORTUNITY

The Green Boulevard

The concept of the Green Boulevard is to create a central green neighborhood link along Willets Point Boulevard that connects all major public open spaces in the District and is itself a showcase of the best practices for streetscape design. It is an opportunity to use a distinctive design and material palette that incorporates lush landscapes, such as a double row of trees and integrated plantings throughout. The Green Boulevard is also envisioned as the central corridor of sustainable utilities and stormwater.



KEY PLAN



Figure 10. Illustrative view of the Green Boulevard
The above image depicts the intent of the design guidelines and is for illustrative purposes only



Lushly planted streetscapes in Washington, D.C.



3. Subarea Design Guidelines:

AREA B

Area B guidelines are organized into the following subsections:

3.5	RESIDENTIAL STREETS	119
3.6	NEIGHBORHOOD PARK	127
3.7	GREEN EDGES	131
3.8	CONVENTION CENTER	139



Area B is envisioned as a sustainable mixed-income neighborhood that is centered around a neighborhood park. Located in the restricted height zone and outside of the stadium entertainment area, the neighborhood provides a variety of housing types within an intimately-scaled urban fabric located along quiet, friendly, pedestrian-oriented streets.

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Residential Streets

The Residential Streets create a sense of community for Willets Point. Entrances to individual units and clusters of street furniture animate the sidewalk and create human scale. Robust, sustainable landscapes make each Residential Street a green link to the Neighborhood Park, which is within a block from most residential buildings in Willets Point.

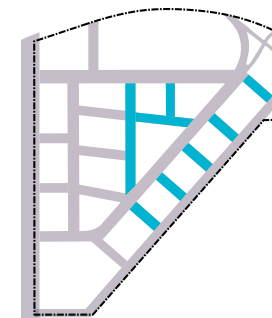


Figure 1. Diagram of the Residential Streets
The image at left depicts the intent of the design guidelines and is for illustrative purposes only

3.5 RESIDENTIAL STREETS Overview

INTIMATE NEIGHBORHOOD STREETS

The Residential Streets are the urban fabric of the residential neighborhood. They are envisioned to be short blocks with an intimate neighborhood feel and scale. Friendly, animated facades have frequent individual front doors and bay windows. Streetscapes are envisioned as pedestrian and family-oriented with locations for social seating and sustainable stormwater management that lead to the Neighborhood Park. Upper stories provide great views of the park and waterfront beyond.

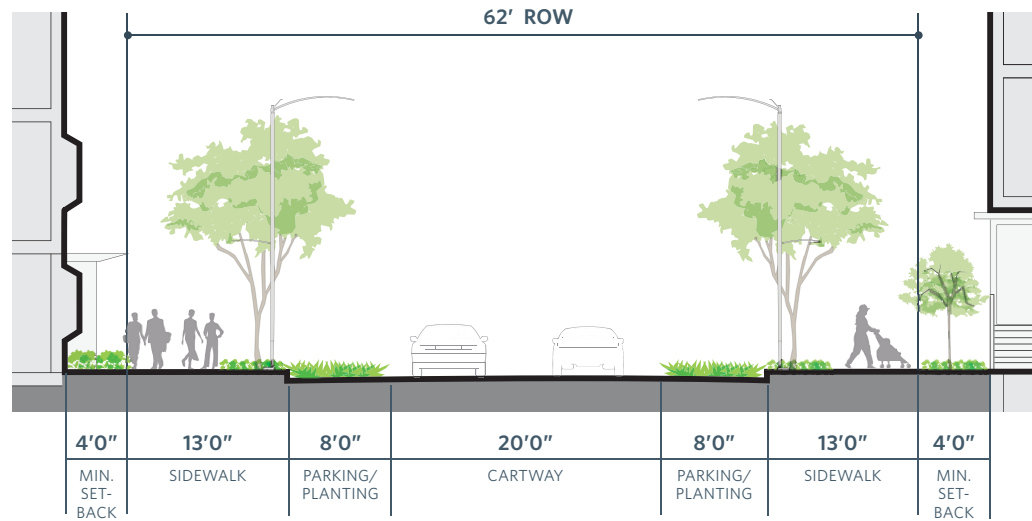
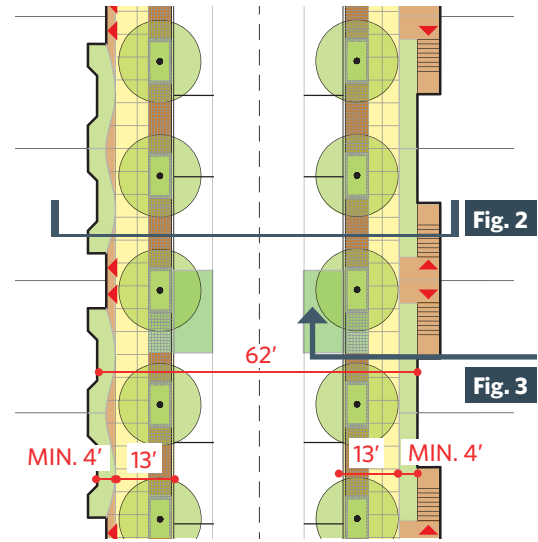
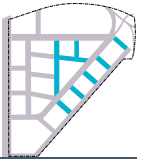


Figure 2. Illustrative plan and section of a typical Residential Street per SZD dimensional requirements

The above image depicts the intent of the design guidelines and is for illustrative purposes only



PUBLIC REALM GUIDELINES

See pages 122-123 for details.

Intimately-scaled Residential Streets are defined by their robust sustainable landscapes and an active sidewalk that supports a pedestrian-friendly urban environment.

3.5.1 Animated Streets

3.5.2 Sustainable Streetscapes

ARCHITECTURE GUIDELINES

See pages 124-126 for details.

Building facades on Residential Streets are lively edges that are articulated with individual entrances, planted setbacks, and an animated and varied streetwall.

3.5.3 Multiple Entrances on the Ground Floor

3.5.4 Animated Lower Base

3.5.5 Planted Setback Areas

3.5.6 Varied Street Wall



Figure 3. Rendering depicting the relevant Design Guideline topics
The image at left depicts the intent of the design guidelines and is for illustrative purposes only

3.5 RESIDENTIAL STREETS Public Realm Guidelines

For Guidelines regarding issues common to all streetscapes and open spaces, see Chapter 2 Design Quality Standards: Public Realm.

3.5.1 Animated Streets

Creating a sense of community, these intimate Residential Streets should be designed to foster an animated and pedestrian-friendly urban environment that is akin to an outdoor living room for all the residents on any particular street.

- Design the streetscape with ample opportunities for sun-exposed and shaded public seating to encourage social activity.
- Provide other streetscape elements such as bike racks, trash receptacles and lighting at frequent intervals to support a pedestrian-friendly street environment.
- While on-street parking should be provided on all Residential Streets in order to create an active and safe environment, at certain key locations such as corner intersections or mid-block locations, consider expanding the sidewalk in place of parking to create a more generous pedestrian area. See Figure 4.
- At intersections, terminate the parking lane at least 35' from the curb where feasible to create an expanded pedestrian area. See Figure 5.
- Animate these expanded pedestrian areas with social seating, additional plantings or trees and other desirable streetscape amenities.
- Contiguous on-street parking is recommended not to exceed 4 spaces without the introduction of an expanded pedestrian green area.
- Although allowed per the SZD, avoid locating curb cuts for garage entries on these streets. Instead locate them on the Perimeter Streets.



Figure 4. Rendering depicting expanded pedestrian areas that can become animated nodes of additional streetscape amenities

The above image depicts the intent of the design guidelines and is for illustrative purposes only

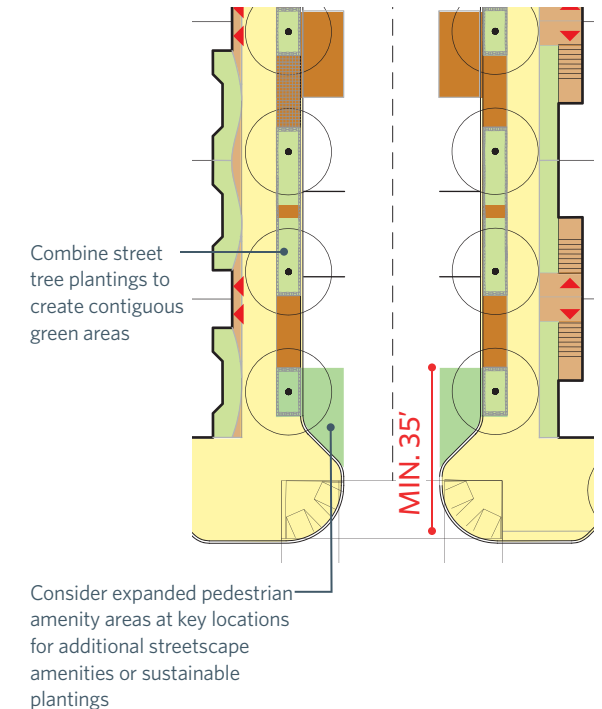


Figure 5. Illustrative Plan of a typical Residential Street

The above image depicts the intent of the design guidelines and is for illustrative purposes only



3.5.2 Sustainable Streetscapes

The Residential Streets are an opportunity to create intimate landscaped streets that can incorporate sustainable strategies for stormwater systems.

- Coordinate plantings in Expanded Sidewalks and along the Sidewalk Furnishing Zone in order to maximize landscape areas, providing a green edge to the sidewalk.
- In key locations, consider expanding the sidewalk in place of parking to create an area for site-wide stormwater systems. These may be in the form of low-impact development (LID) techniques, such as permeable paving systems that connect to underdrains with plantings for stormwater management. See *Figure 6*.



Figure 6. Rendering depicting LID systems that are incorporated into the parking lanes on the Residential Streets

The above image depicts the intent of the design guidelines and is for illustrative purposes only



The sidewalk should be designed with plantings throughout to provide a green environment and complement individual ground floor residential units

3.5 RESIDENTIAL STREETS Architectural Guidelines

For Guidelines regarding issues common to all building facades, see Chapter 2 Design Quality Standards: Architecture.

3.5.3 Multiple Entrances on the Ground Floor

Per the SZD, each ground floor dwelling unit on a Residential Street shall be accessed directly from the street. The predominant ground floor character of all Residential Streets should be individual units that provide a human scale at the building base and create an enlivened streetscape.

- Major lobby entrances are not intended to be on these streets. Secondary building entrances may be provided on these streets but limited to a maximum of **20' in length**.
- Per the SZD, parking garage entries are allowed on these streets but should be integrated into the facade to create a friendly and safe environment and limited to a maximum of **20' in length**.
- On buildings that also front Perimeter Streets or Service Streets, provide access to parking garages from those streets and not Residential streets.

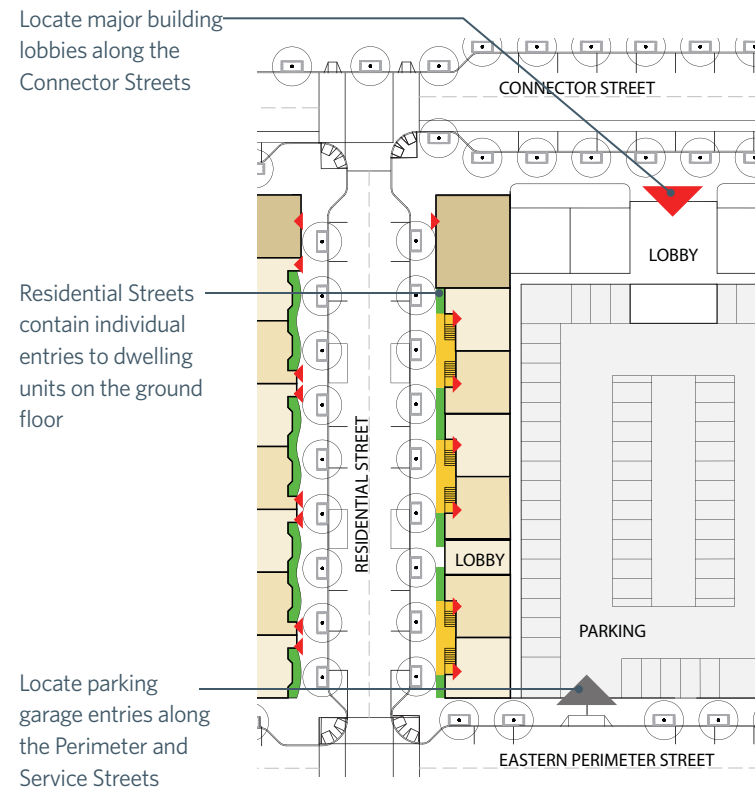
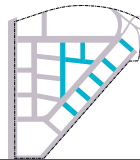


Figure 7. Plan diagram depicting a typical ground floor layout
The above image depicts the intent of the design guidelines and is for illustrative purposes only



Figure 8. Illustrative view depicting an animated lower base of a residential building
The above image depicts the intent of the design guidelines and is for illustrative purposes only



3.5.4 Animated Lower Base

The character of the lower base is key in defining the animated pedestrian street experience envisioned for the Residential Streets. The lower floors along the entire length of the street should be designed to create a strongly defined public space, articulated through residential entry stoops and gardens, bay windows, balconies and other features that provide a lively sense of activity and safety, connect indoor and outdoor spaces and complement the streetscape design.

- Use volumetric manipulations, projections, recesses, and variety in materials to create a pronounced lower base that relates in an immediate manner to activity at the street level.
- Expression of individual entrances through the use of awnings, canopies, bay windows or recesses etc. is encouraged. Pedestrian-scaled lamps may be provided in the setback zone to demarcate entries.
- Consider the use of distinctive materials in the setback entry area.
- Setback entry areas should not be used for storing garbage or recycling cans.
- Individual units may be accessed at grade or through a raised stoop. Use of stoops is encouraged as it accentuates individual entries, provides a human scale to the facade and creates a social space that fosters the sense of community in the neighborhood.
- Limit stoops to a **maximum of 4' above the ground**. Portions of facades that are below these raised stoops need to be designed in a manner that incorporates windows, screens or plantings to avoid blank and inactive facades.
- Integrate the design of stair and stoop railings with the overall language of the building and coordinate with railings elsewhere in the building.

DISCOURAGED:



Avoid blank, window-less facades and stoops that are raised higher than 4'



Avoid facades that don't provide entries to individual units at the ground floor

PREFERRED OPTIONS:

ARTICULATED BASE OF BUILDINGS



Articulated buildings that employ variety in materials and volumetric manipulations create a human scale at the lower base

ANIMATED ENTRIES AT-GRADE AND RAISED STOOPS



Individual entrances on the ground floor should be articulated through use of steps, stoops, canopies, bay windows, plantings etc. to create animated street facades

3.5 RESIDENTIAL STREETS Architectural Guidelines

For Guidelines regarding issues common to all building facades, see Chapter 2 Design Quality Standards: Architecture.

3.5.5 Planted Setback Areas

Per the SZD, all open areas within the setback zone that front residential units need to be planted except for zones used for entries to individual units.

- Maximize continuous planted areas in the setback zone by grouping entries to ground floor dwelling units.
- Use plantings creatively to provide screens to inactive facade areas, especially in instances of raised entries to dwelling units.
- Planted areas may be designed to be flush-to-grade planting beds, hedges or raised planters.
- If the setback area provided for planting is more than 4', consider planting small trees.
- Select plantings that will add color and provide year-round greenery.
- Avoid planter walls more than 18" in height.
- Fenced edges are discouraged.



The planted setback area should contribute to the vitality of the streetscape and provide space for a personal garden at each entrance

3.5.6 Varied Streetwall – Upper Base

Facades of buildings on these streets should be lively edges that incorporate variation in building materials and integrate architectural elements such as balconies, bay windows, etc.

- Design facades with recesses, projections, and balconies in strategic and creative ways to foster an interactive environment as well as to reduce the perceived scale of long facades.
- As permitted by SZD, where possible, setback building facades at a height of 40' (instead of 60') to provide a more human scale at the base and variation in the streetwall.
- Incorporate outdoor terraces and balconies that can enjoy views to the Neighborhood Park, especially on park-facing Residential Streets.



The upper base should be articulated with varied massing and balconies to animate the street

Neighborhood Park

The Neighborhood Park is the heart of the residential neighborhood. It is envisioned as less than a block's walk from most residential buildings in Willets Point. It balances passive and active spaces to form a frequent destination for residents and visitors of all ages.

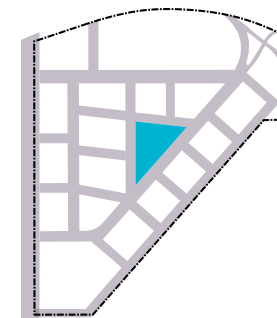


Figure 1. Diagram of the Neighborhood Park
The image at left depicts the intent of the design guidelines and is for illustrative purposes only

3.6 NEIGHBORHOOD PARK Overview

THE HEART OF THE NEIGHBORHOOD

The Neighborhood Park is the most significant green area in Willets Point and the heart of the residential area. It is envisioned as a focal point along Willets Point Boulevard. Serving primarily the resident population, the Park should include active, semi-active and passive spaces for all age groups with strong and porous connections to all residential streets as well as the school and community facility.

Figure 2, shown to the right, represents one of many possible layouts and is shown for illustrative purposes only to demonstrate programmatic and site planning goals, as described on the following pages.



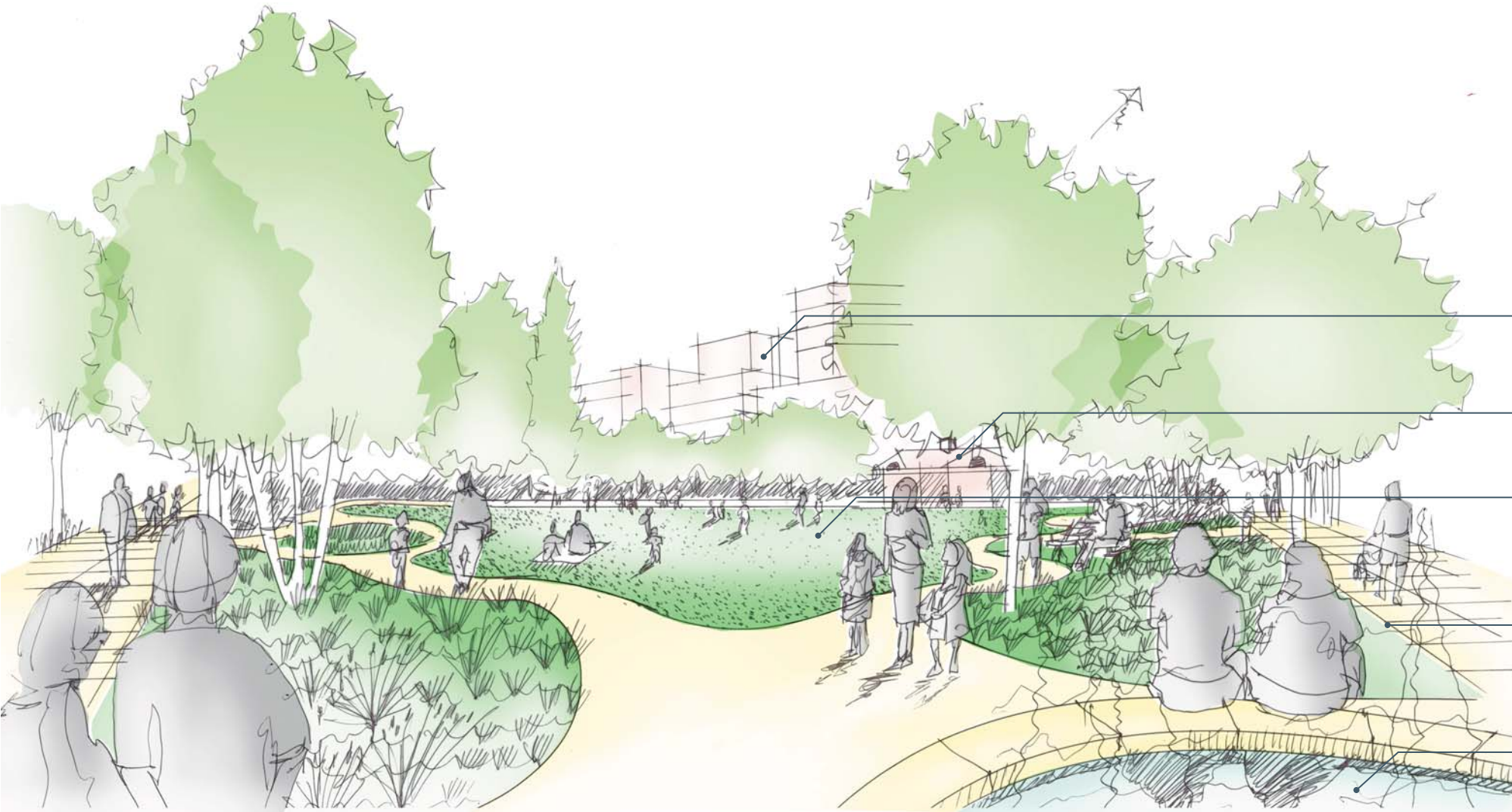
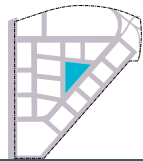
Figure 2. Illustrative diagram of the Neighborhood Park
 The above image depicts the intent of the design guidelines and is for illustrative purposes only

PUBLIC REALM GUIDELINES

See page 130 for details.

The park should provide a sustainable landscape with activities and spaces for all ages.

- 3.6.1 Sustainable Landscape Identity**
- 3.6.2 Balance of Active and Passive Spaces**
- 3.6.3 Permeable Edges and View Corridors**
- 3.6.4 Buildings and Structures**
- 3.6.5 Links to School and Community Facilities**



Locate the park in close proximity to the school or community center

Include a park field house that is accessible from all areas of the park

Provide both active and passive recreational spaces

Design park edges for visual permeability with entrance points at all street intersections

Consider integrating sustainable water features into the park

Figure 3. Illustrative sketch of the Neighborhood Park
The above image depicts the intent of the design guidelines and is for illustrative purposes only

3.6 NEIGHBORHOOD PARK Public Realm Guidelines

For Guidelines regarding issues common to all streetscapes and open spaces, see Chapter 2 Design Quality Standards: Public Realm.

3.6.1 Sustainable Landscape Identity

The park should have a recognizable landscape identity that can become a signature of the neighborhood.

- Incorporate visible sustainable strategies in strategic ways, such as special water features at entry points and within the park.
- Consider introducing topography in the park to enrich the experience and heighten the sense of scale.



Sustainability features can be integrated in interesting ways into active and passive play spaces

3.6.2 Balance of Active & Passive Spaces

The park should be designed for use by all age groups and include a balance of active, semi-active, and passive spaces.

These spaces may include the following:

- Multi-use fenced sport courts: basketball, handball, skateboarding, in-line skating, etc.
- Open lawn areas for informal soccer, football and frisbee play as well as for picnicking or sunning.
- Pathways with shaded seating.
- Paved plaza area with tables and chairs for lunchtime, concession dining or cultural events.
- Entry gateway with a fountain or feature and informal seating.
- Playground or tot lot with seating.



Balance active and passive uses in the park

3.6.3 Permeable Edges & View Corridors

The park is the largest public open space in the district. It should be an open and welcoming center of the neighborhood with strong connections to all areas of the neighborhood as well as the school and community facility.

- Design the park edge to have permeable entry points, or “green links”, at all pedestrian connections of street intersections.
- Integrate entry points into the edge design and consider including highly visible features such as sculpture.
- Provide continuous visual connections into and out of the park along all non-accessible edges.
- Limit all non-accessible edges in height by using designs such as curbs with low fences, low berms under 3’ in height, and/or trees and low plantings.
- The park should be open to the public according to standard NYCDPR regulations.



Design entry points to be highly visible, accessible, and marked by a special feature

3.6.4 Buildings & Structures

The park is encouraged to include a comfort station or concession structure that provides bathrooms, maintenance facilities and a small concession.

The building should be centrally located to all areas of the park and be architecturally integrated into the design and landscape of the park.

3.6.5 Links to School & Community Facility

The park and the school and community facility are envisioned to work together, creating a central public heart of the neighborhood. A direct connection is encouraged between these areas.

In addition, areas of the park such as playgrounds and sports fields may be shared by the school or community facility.



Areas of the park, especially sports facilities, may be shared with the adjacent school

Green Edges

The Green Edges create an identity for the highly visible perimeter of the Willets Point site. At the same time they shield passers-by from views of parking, service areas, and other back-of-house functions. They absorb the change in grade to the new street elevation and enable possible future expansion of the district.

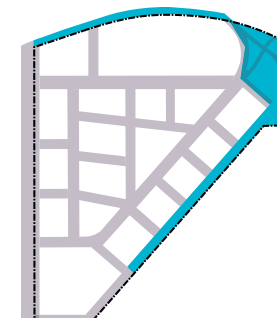


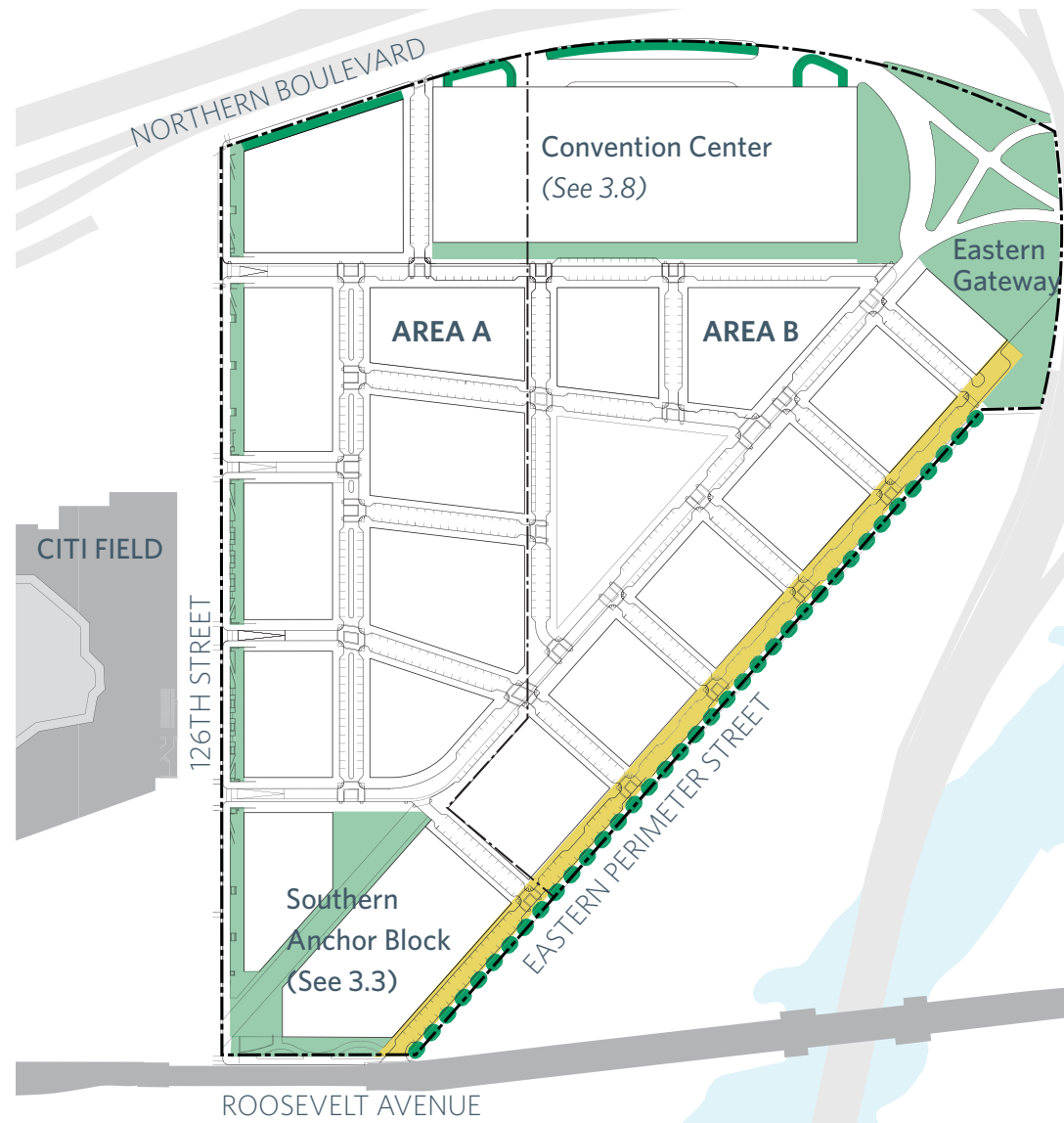
Figure 1. Diagram of the Green Edges
The image at left depicts the intent of the design guidelines and is for illustrative purposes only

3.7 GREEN EDGES Overview

GREEN EDGES OF THE DISTRICT

Green Edges refers to the Perimeter Streets of the District that are intended to portray a green and sustainable character for Willets Point. The buildings and landscapes along these streets will be visible from adjacent neighborhoods, especially Flushing and are intended to provide a dynamic and architecturally stimulating skyline and environment.

Connecting 126th Street to the Eastern Gateway, these Green Edges are also great opportunities for sustainable stormwater management, to mitigate pollution from the adjacent highways, shield parking and service areas and provide space for a grade change between new and existing elevations.



- Legend
- ● ● Landscape Buffer
 - Eastern Perimeter Street
 - Open Space Areas



Figure 2. Diagram showing the relevant Design Guidelines topics
The above image depicts the intent of the design guidelines and is for illustrative purposes only



PUBLIC REALM GUIDELINES

See page 136 for details.

The streetscapes are intended to be of the same high-quality standards found throughout the District.

3.7.1 Perimeter Landscape Buffer

3.7.2 Welcoming Eastern Gateway

See also, *Enhancement Opportunity: Eastern Gateway Green*, page 138

3.7.3 Curb cuts for Parking and Service

3.7.4 Roadway and Parking Lane

ARCHITECTURE GUIDELINES

See page 137 for details.

The buildings are encouraged to provide active facades that face outward towards the adjacent neighborhoods.

3.7.5 Active Ground Floor Character

3.7.6 Integrated Parking Garages

3.7.7 Varied Skyline



Figure 3. Perspective view of Eastern Perimeter Street

The above image depicts the intent of the design guidelines and is for illustrative purposes only

3.7 GREEN EDGES

NORTHERN BOULEVARD

Although pedestrian access is limited along Northern Boulevard, the streetscape provides a critical link from 126th Street to the Flushing Promenade. A landscape buffer is intended to mitigate pollution from the highways as well as shield exposed parking at the lower floors. Along the Convention Center, the buffer also shields the view of the loading area. See Figures 4 and 5 and also 3.8 Convention Center.

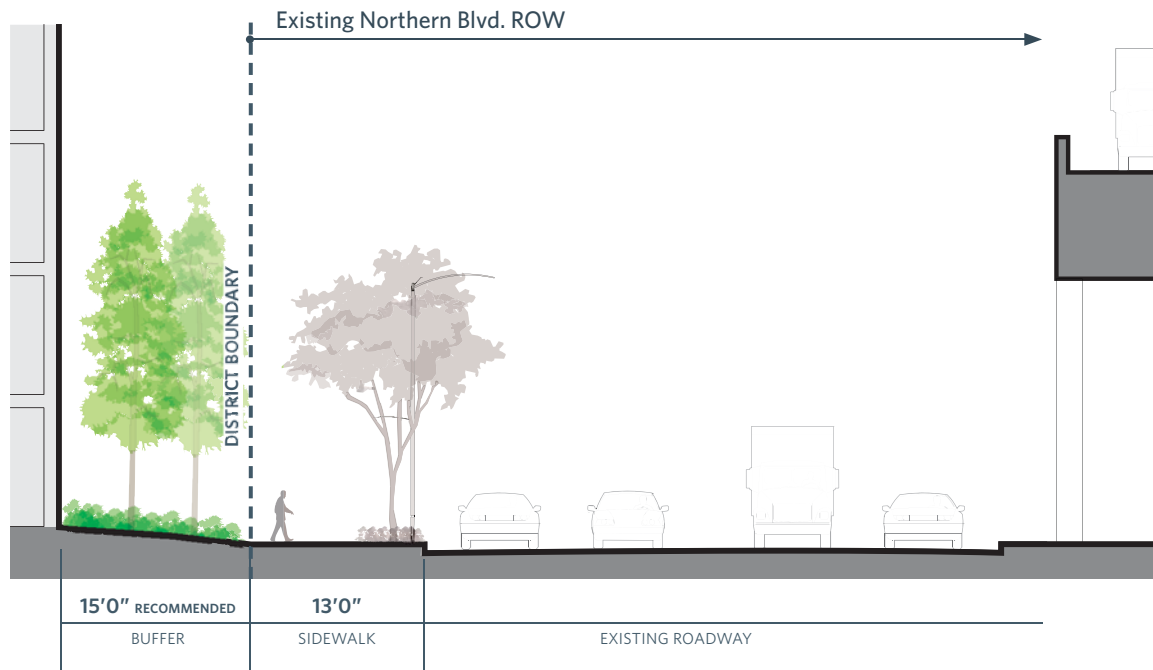
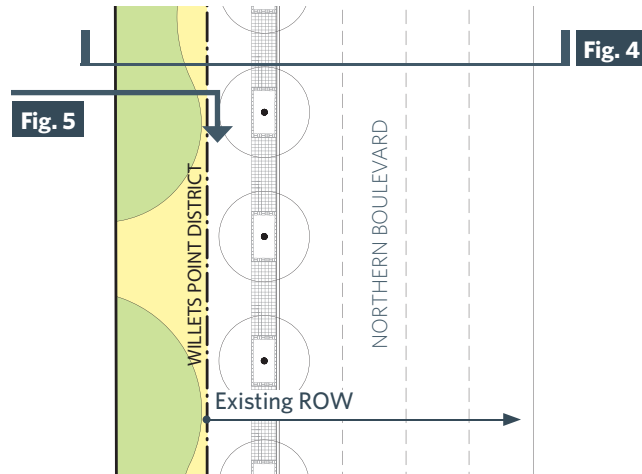
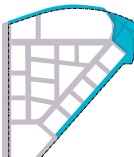


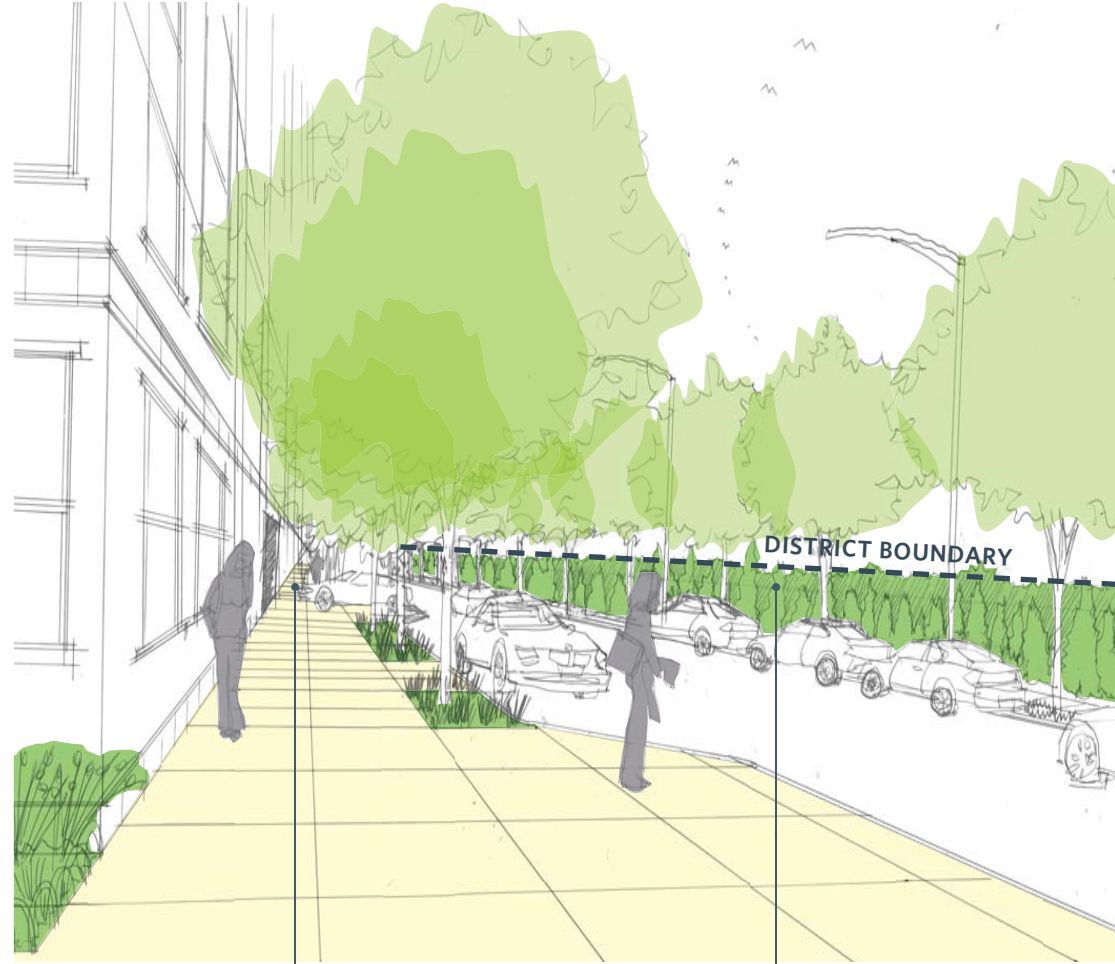
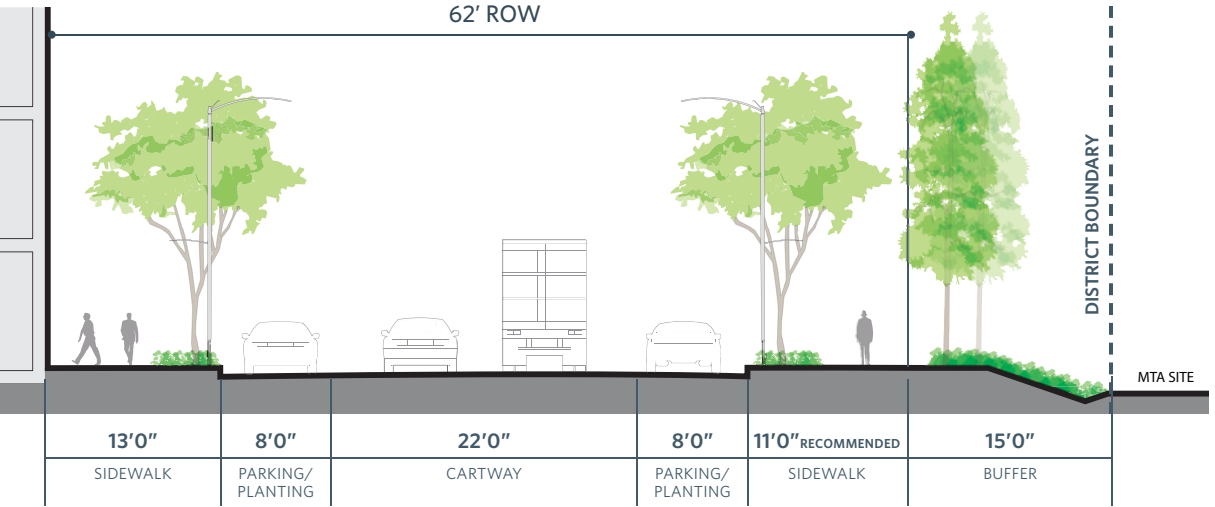
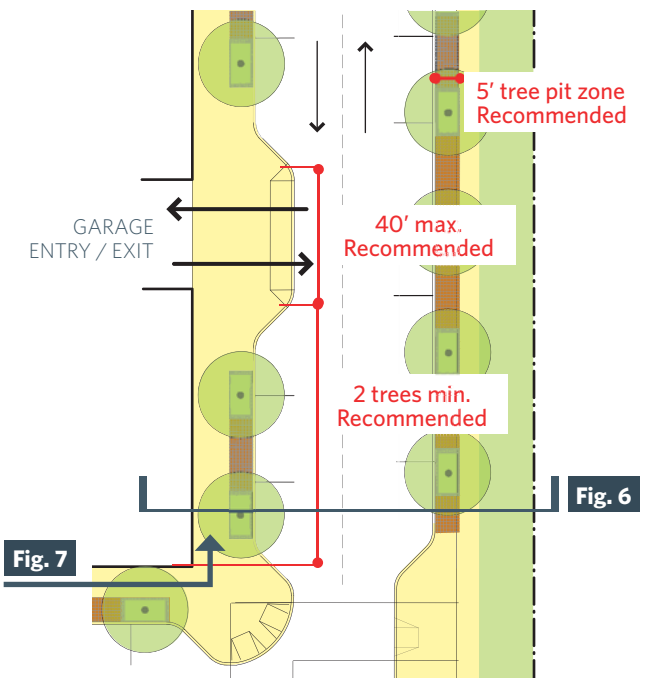
Figure 4. Typical plan and section of Northern Boulevard per SZD dimensional requirements
 The above image depicts the intent of the design guidelines and is for illustrative purposes only

Figure 5. Rendering of Northern Boulevard showing the relevant Design Guidelines topics
 The above image depicts the intent of the design guidelines and is for illustrative purposes only



EASTERN PERIMETER STREET

The Eastern Perimeter Street will be very visible from Flushing and is intended to have a visibly green edge. Although it is recommended that garage and loading occur on this street rather than the Residential Streets, the streetscape should be of similar pedestrian-oriented standards found throughout the District. Street trees should be provided on the far side of the street to allow them to mature at the same rate as those along the building frontage. See Figures 6 and 7.



Design pedestrian-friendly garage entrances that maintain a continuous sidewalk elevation

Use buffer plantings to shield the view of parking garages and provide a green edge to the District

Figure 6. Typical plan and section of Eastern Perimeter Street per SZD dimensional requirements

The above image depicts the intent of the design guidelines and is for illustrative purposes only

Figure 7. Rendering of Eastern Perimeter Street showing the relevant Design Guidelines topics

The above image depicts the intent of the design guidelines and is for illustrative purposes only

3.7 GREEN EDGES Public Realm Guidelines

For Guidelines regarding issues common to all streetscapes and open spaces, see Chapter 2 Design Quality Standards: Public Realm.

3.7.1 Perimeter Landscape Buffer

Per the SZD, an 8'-15' (15' preferred) wide landscape buffer should be provided along the building frontage on Northern Boulevard and along the eastern boundary of Eastern Perimeter Street.

- **NORTHERN BOULEVARD:** Provide a continuous landscape buffer along the building frontage except at required entrances and exits. Design the buffer to effectively shield the parking garage facades and loading areas and mitigate air pollution. *See also 3.8 Convention Center for additional guidelines.*
- **EASTERN PERIMETER:** Provide a continuous landscape buffer along the sidewalk that is designed to provide year-round greenery and an effective shield to the MTA lot (should it remain undeveloped).
- Select plantings to provide year-round greenery that provides a green edge to the District as seen from afar.
- Design the buffer as an integral part of the streetscape that maintains a high-level of pedestrian safety and visibility, avoiding the creation of dark and hidden spaces.
- The buffers are also the primary device for negotiating the grade change between existing elevations and the district grade levels. They may be sloping or terraced but should not have fences or planter walls above 2' in height.
- Due to their linear nature and connection to the Eastern Gateway area, the buffers are an opportunity for sustainable stormwater management, such as bio-swales.

3.7.2 Welcoming Eastern Gateway

The Eastern Gateway connects Willets Point to the region.

- Design this gateway as a welcoming entrance into the District that integrates the new ramps into a pedestrian-oriented District entrance. *See also 3.4 Connector Streets.*
- Consider creating sustainable landscapes throughout the undeveloped land areas to create a green gateway.
- Provide landscapes around the building fronts facing the gateway to surround the District in greenery, connect with the landscape buffers along Eastern Perimeter Street and Northern Boulevard, and reinforce the green gateway.
- *See also Enhancement Opportunity: Eastern Gateway Green.*



Buffer plantings provide a year-round shield as well as an opportunity for sustainable stormwater management

3.7.3 Curb cuts for Parking & Service

Eastern Perimeter Street only

Vehicular entries should be located along the Eastern Perimeter Street in order to allow the adjacent Residential Streets to remain as pedestrian and family-friendly as possible. However, all curb cuts should prioritize the pedestrian and create a safe sidewalk.

- Design curb cuts to maintain a continuous sidewalk elevation.
- Seek to limit curb cuts for garage access to an **aggregate width of 40'**, instead of 60' as allowed per the SZD. Where a loading dock is required, the aggregate curb cut may be 60' per the SZD.
- Locate curb cuts a sufficient distance from the corner to allow for at least 2 street trees, placed 25' apart, between the curb cut and the street corner.

3.7.4 Roadway and Parking Lane

Eastern Perimeter Street only

Although the Eastern Perimeter Street is to be developed on only one side, it should be designed to function as a pedestrian-oriented neighborhood street in addition to a service corridor. This design allows for the integration of future development on the other side of the street.

- Although the SZD allows for up to 3 lanes of traffic, it is recommended that a maximum of 2 lanes be provided. This guideline will help ensure that the street remains pedestrian-friendly and maintains an urban character consistent with the District.
- The SZD allows for the provision of either a parking lane or Pedestrian Amenity Area. The provision of parking on both sides of the street is encouraged to maintain a safe and active neighborhood street.

Architectural Guidelines

For Guidelines regarding issues common to all building facades, see Chapter 2 Design Quality Standards: Architecture.

3.7.5 Active Ground Floor Character

Although the Perimeter Streets are the preferred location for back-of-house facilities, these elements should be mitigated and integrated into the facade to create a friendly and safe environment.

- The required corner uses of residential, community, or commercial uses shall be at least 25' per SZD but are recommended to be **35'** to allow for a residential unit. Provide windows and/or doors at the ground floor to provide an active streetwall.
- At streetwall parking garages, provide active uses such as bike storage or other uses on the ground floor. Provide windows and pedestrian access (where appropriate) to minimize the amount of inactive facade at the ground floor. Consider designing spaces within the garage at the streetwall to allow for the possible future conversion to other active uses such as retail.
- Visually integrate service doors into the facade using high-quality materials consistent with the rest of the facade. Consider separating and dispersing service doors around the perimeter of the building as much as possible to prevent a long sequence in one location.
- Provide pedestrian-oriented facade lighting to create a safe night-time environment.



Successful example of a garage entry integrated into an active facade with windows



Example of a long sequence of poorly integrated service doors and grilles that create an unfriendly streetscape

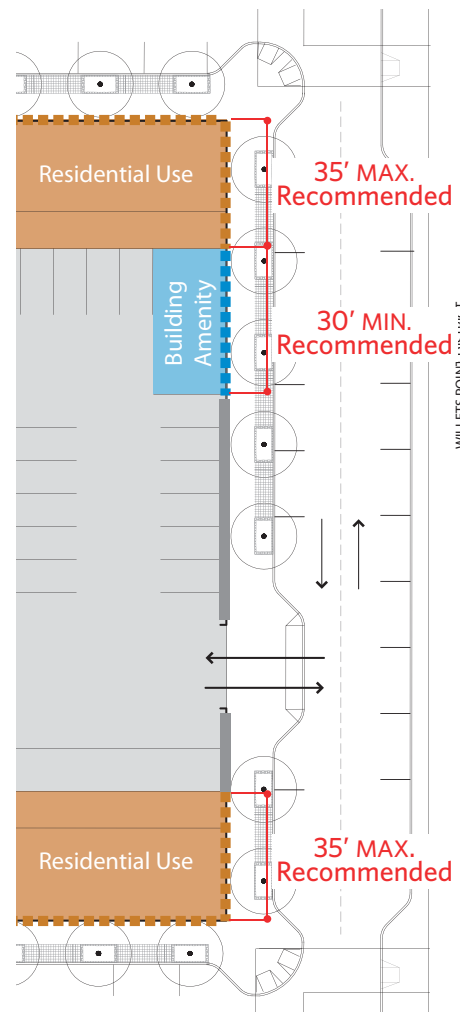


Figure 8. Diagram of a typical block along Eastern Perimeter Street
The above image depicts the intent of the design guidelines and is for illustrative purposes only

3.7.6 Integrated Parking Garages

The streetwall containing the parking garage shall be designed to maximize integration of the parking garage into the overall facade in order to minimize its overall qualitative impact. See also Chapter 2 Design Quality Standards: Architecture.

- Clad the permitted 40' in height of streetwall parking garage using materials, colors, and textures that integrate with the rest of the facade.
- Although screening of the streetwall parking garage is not required on the Perimeter Streets, consider using architectural screens, lighting effects, greenwalls, or other visual devices.

3.7.7 Varied Skyline

The Perimeter Streets should provide a visually dynamic face and skyline to the District as seen from the neighboring areas including Flushing as well as from the adjacent highways and subway.

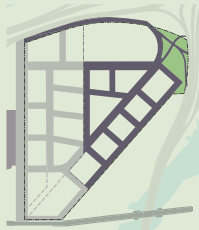
- The Perimeter Streets will contain both Area A and Area B buildings with a range of height limits. Emphasize this variation in the building massing to create a dynamic sequence of buildings and skyline as seen from within the District and from afar.
- Design taller buildings on both Perimeter Streets to take advantage of views from interior spaces to the bay, creek and city fabric beyond.

3.7 GREEN EDGES

ENHANCEMENT OPPORTUNITY

Eastern Gateway Green

The Eastern Gateway Green is a concept for a model combined open space and sustainability facility. Using the land of the northeastern parcels, an innovative stormwater management feature can also be a major public green space. This open space could be a showcase of sustainability for Willets Point, a learning opportunity for the community and the region, a pedestrian recreation area with links to Flushing Creek, as well as a major gateway feature. Examples of open spaces that combine major sustainability functions can be found in Austria (far right, top) and Portland, (far right, bottom).



KEY PLAN



Figure 9. Illustrative view of the Eastern Gateway Green

The above image depicts the intent of the design guidelines and is for illustrative purposes only



Naturalized stormwater management areas can also function as unique public open spaces



Convention Center

The Convention Center will be an icon that is fully integrated into a pedestrian-friendly urban neighborhood. Its porous and active street-front will provide public plazas and functions, while the expansive roof may be an opportunity for a sustainable and/or recreational feature.

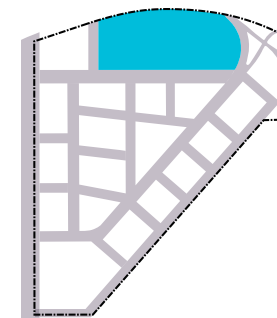


Figure 1. Diagram of the Convention Center

The image at left depicts the intent of the design guidelines and is for illustrative purposes only

3.8 CONVENTION CENTER

Overview

AN URBAN CONVENTION CENTER

The Convention Center at Willets Point will be a neighborhood and a district icon that is completely integrated into the surrounding urban fabric. With an articulated street frontage and a generously landscaped public plaza, it will provide a variety of pedestrian experiences, both for visitors and local residents. The building should be programmed to be active and open year-round in a manner that supports the adjacent retail, entertainment and residential uses.

With a significant surface area on its roof, the Convention Center provides an opportunity to both integrate sustainable features addressing water and energy into the design of the building and provide an elevated open space area and recreational amenity.

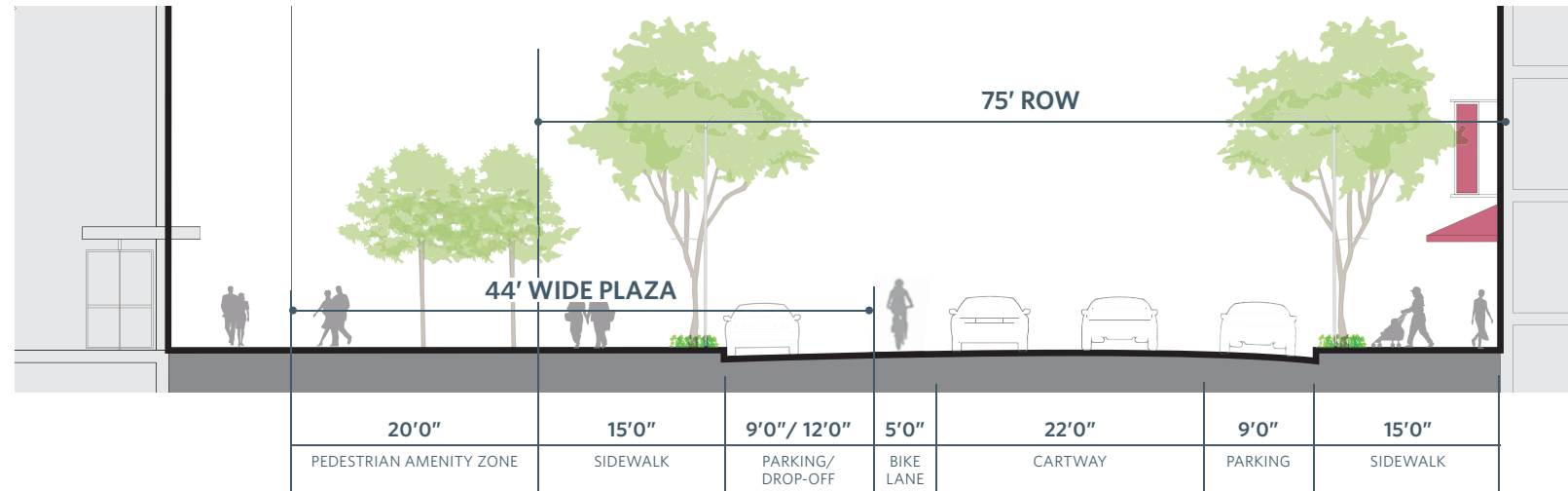
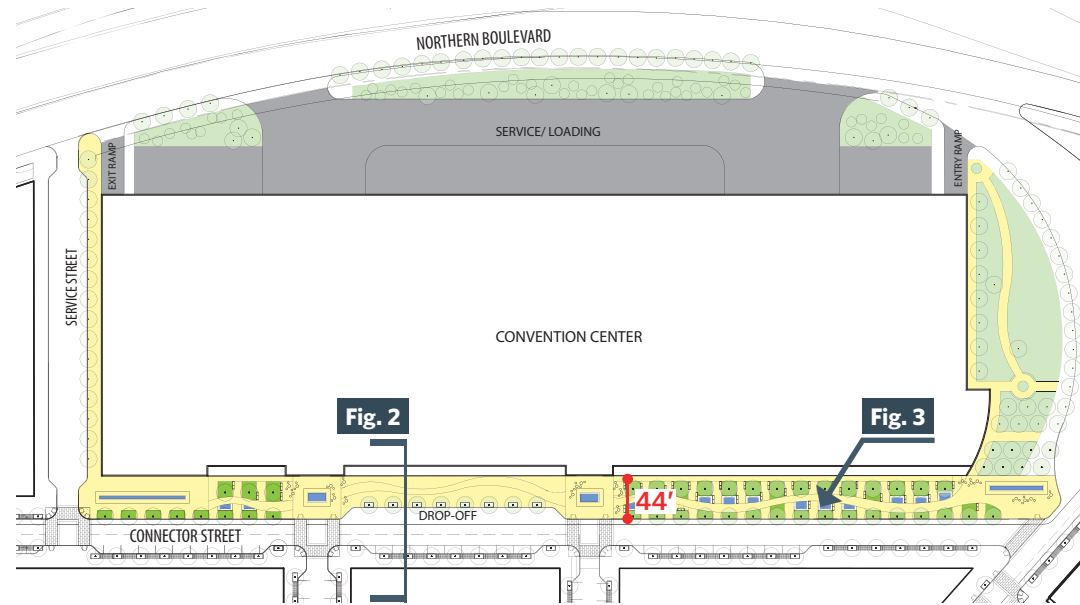
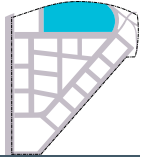


Figure 2. Illustrative plan and section of the Convention Center
 The above image depicts the intent of the design guidelines and is for illustrative purposes only



PUBLIC REALM GUIDELINES

See pages 142–143 for details.

Streetscapes and Plazas fronting the Convention Center are pedestrian-friendly public places that cater to both residents and visitors to the neighborhood.

3.8.1 Convention Center Plaza

3.8.2 Vehicular Access and Drop-off Zone

3.8.3 Landscape Buffer along Northern Boulevard

ARCHITECTURE GUIDELINES

See pages 144–145 for details.

The Convention Center is articulated with a building facade that is lively, transparent and fully integrated into the urban environment of Willets Point.

3.8.4 Urban Integration

3.8.5 Transparent and Articulated Facade

3.8.6 Public Programming

3.8.7 Sustainability Strategies

See also *Enhancement Opportunity: Convention Center Roof*, page 146.



Figure 3. Rendering depicting the Convention Center Plaza
The above image depicts the intent of the design guidelines and is for illustrative purposes only

3.8 CONVENTION CENTER Public Realm Guidelines

For Guidelines regarding issues common to all streetscapes and open spaces, see Chapter 2 Design Quality Standards: Public Realm.

3.8.1 Convention Center Plaza

The Convention Center Plaza should be designed to be a welcoming place that both supports the volume of pedestrian movement associated with the Convention Center and functions as “a public place” for the residents and other visitors to the neighborhood. Incorporating hardscape, landscape and sustainable water features, it should be a memorable space that creates an identity for the Convention Center. See Figure 4.

- Design the plaza to cater to Convention Center visitors as well as other audiences by allowing for easy circulation through as well as offering opportunities for passive activities and small gatherings.
- Design the curbside area to coordinate with the overall language of the 34th street streetscape.
- Consider incorporating distinctive streetscape furniture, a double row of trees, sustainable plantings, public art, banners, etc. to create a memorable plaza.
- Emphasize the western and eastern ends of the Convention Center plaza with distinct elements, such as specialized paving, public art, water feature or other streetscape amenity while accommodating pedestrian movement.
- Provide a multiple sun-exposed and shaded seating opportunities for people having lunch or for people-watching.
- Consider designing the open space to undulate in form, merging it with the architecture of the building to create a multitude of spaces within the plaza.
- Provide Expanded Sidewalks at pedestrian crossings to facilitate pedestrian circulation and wayfinding.
- Consider the use of water in the landscape design as a sustainable feature.



Melbourne Convention Center



Oregon Convention Center



Montreal Convention Center

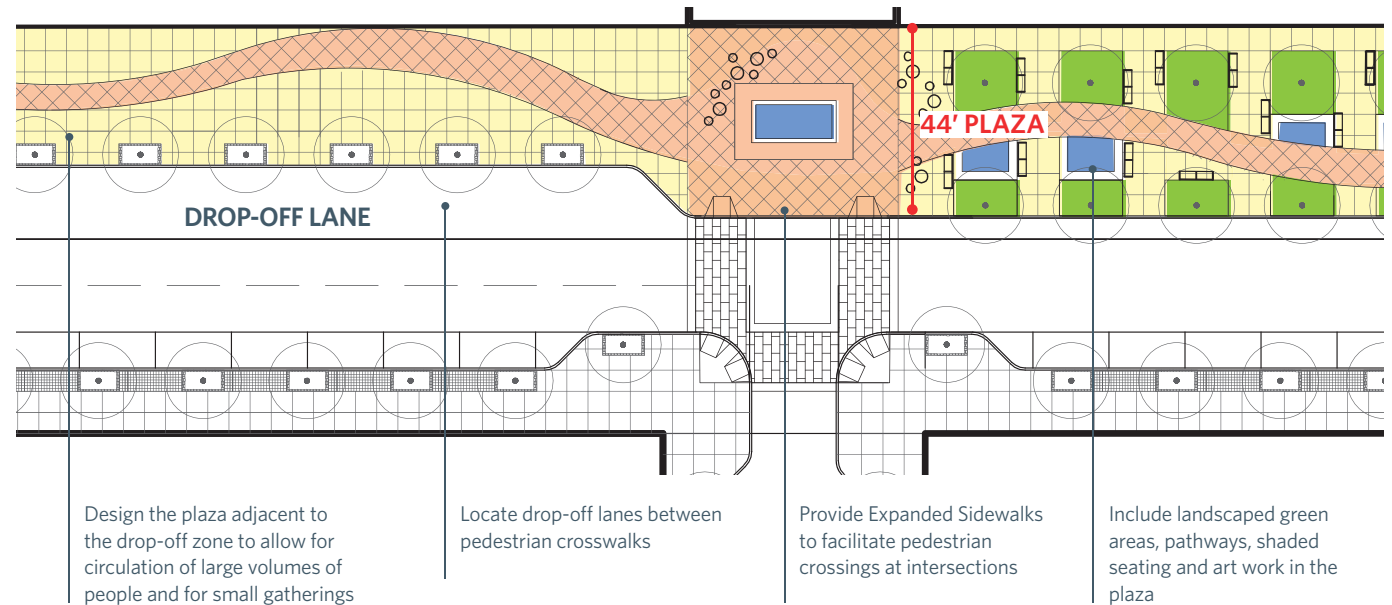
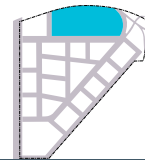


Figure 4. Plan Diagram depicting the plaza and the drop-off lane in front of the Convention Center
The above image depicts the intent of the design guidelines and is for illustrative purposes only



3.8.2 Vehicular Access and Drop-off Zone

Per the SZD, the 9' parking lane should be replaced by a 12' wide drop-off lane in front of the Convention Center. The maximum aggregate length of the drop-off should not exceed 300'.

- The primary entrance of the Convention Center is intended to be on the Connector Street. Drop-off lanes may be provided on the Connector Street but they should be limited to zones between pedestrian crosswalks, so that they don't interfere with pedestrian circulation. See Figure 4.
- Curb cuts are not allowed on the Connector Streets per the SZD. Locate all service and loading access and egress along Northern Boulevard or the Service Street.

3.8.3 Landscape Buffer along Northern Boulevard

Northern Boulevard provides convenient vehicular access to service the needs of the Convention Center. However it should also provide a landscaped edge and allow for safe pedestrian circulation.

- Per the SZD, provide a landscape buffer along the Convention Center frontage of Northern Boulevard. This buffer is recommended to be at least 15' wide and should provide year-round greenery that effectively shields views into the service and loading areas.
- Minimize curb cuts for service/loading entry and egress to allow for a continuous sidewalk along Northern Boulevard.
- Provide access to the parking garage for the Convention Center along Northern Boulevard aggregated with the service/loading entry and egress.



Buffer plantings provide a year-round shield as well as an opportunity for sustainable stormwater

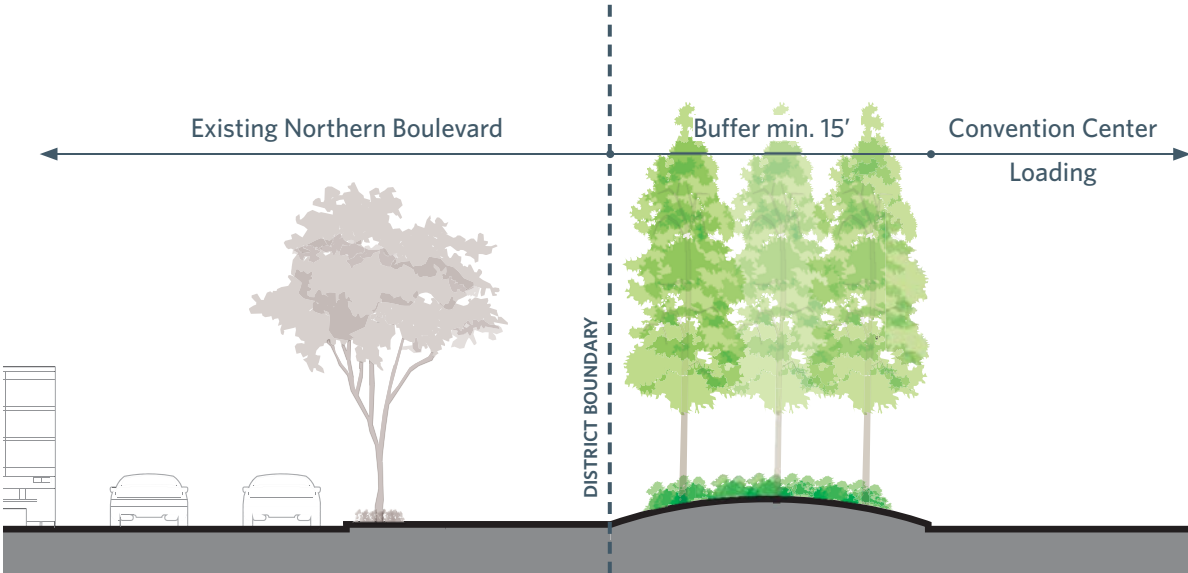


Figure 5. Section depicting the buffer along Northern Boulevard
The above image depicts the intent of the design guidelines and is for illustrative purposes only

3.8 CONVENTION CENTER Architectural Guidelines

For Guidelines regarding issues common to all building facades, see Chapter 2 Design Quality Standards: Architecture.

3.8.4 Urban Integration

The Convention Center at Willets Point is intended to be an icon that is fully integrated into its urban fabric. It is not intended to be a monumental gesture that feels out of scale with the neighborhood. The design of the building needs to be consistent with and reflective of the scale and character of its context and create a regional destination that is a part of an urban neighborhood.

Per the SZD, a Convention Center shall be located on a Connector Street and entirely within 600' from Northern Boulevard. Proximate to several major arterial highways and a transit corridor, the Convention Center is ideally positioned to take advantage of the extraordinary access and visibility in the metropolitan region.

CONNECTOR STREET

The Connector Street facade is intended to be the primary front face of the building. Adjacent to the Area A retail and entertainment uses, the western corner of the building should be designed as a beacon that is an extension of the shopping and dining experience of Willets Point. See Figure 6.

- If the Northern Anchor Block contains a hotel, consider providing a connection between the Convention Center and the Northern Anchor Block to facilitate the sharing of facilities. See Figure 6.
- Provide an architectural emphasis and focal point at the western and eastern ends of the building, that respectively connect to the Area A retail and entertainment district and the eastern gateway.

EASTERN GATEWAY

The eastern face of the Convention Center is a prominent facade that will be highly visible from the surrounding elevated freeways and the ramps leading to the Willets Point eastern gateway. Design the building to be welcoming and memorable and contribute to the experience of the gateway that is envisioned for this corner of the district. See Figure 6.

NORTHERN BOULEVARD

Northern Boulevard is seen as the preferred location for back-of-house, loading, service and parking facilities. Mitigate these elements by integrated them into the facade to create a friendly and safe environment.

For further details see 3.4 Connector Streets and 3.7 Green Edges.



Palais des Congres, Montreal



Convention Center, Boston



Convention Center, Charlotte

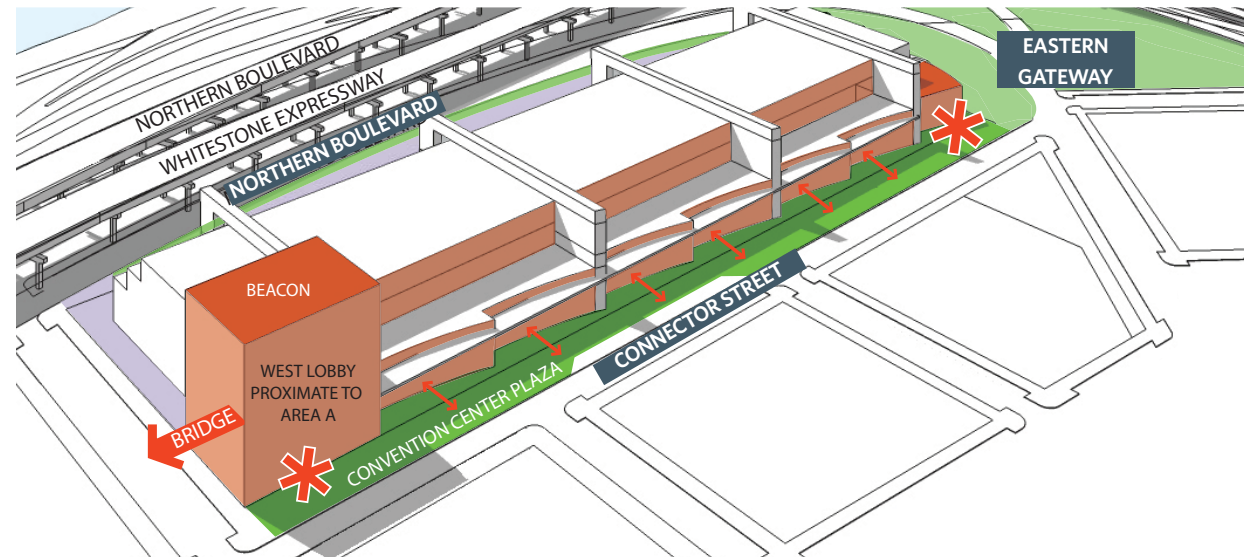
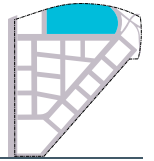


Figure 6. Diagram depicting the siting criteria of the Convention Center
The above image depicts the intent of the design guidelines and is for illustrative purposes only



3.8.5 Transparent and Articulated Facade

The facade of the Convention Center should be vibrant and active and not feel like a “box with docks”, especially as it addresses its main urban face along the Connector Street.

- Design the primary facade with a high level of transparency and variation in materials and massing to humanize its scale.
- While there might be one or two primary destination entries to the building, consider providing multiple secondary entries that can activate the facade along the Connector Street.
- Locate and design public areas of the building such as the lobby and the pre-function zone to be accessed seamlessly from the plaza and feel like an extension of the outdoor pedestrian areas.



Icahn Lab, Princeton

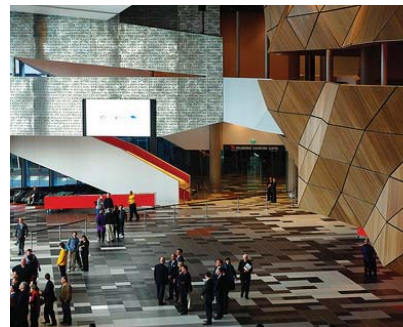


Convention Center, Phoenix

3.8.6 Public Programming

The Convention Center should remain open year-round and be programmed with active uses that can operate independent of exhibitions and expositions within the building.

- Consider making the lobby and pre-function areas along the Connector Street open to the general public. These spaces can integrate permanent galleries and art collections to add value and be a draw for non-convention audiences as well.
- Incorporate retail uses such as cafes and restaurants into the public portions of the buildings so that the Convention Center can serve both residents and visitors to the District.



Melbourne Convention Center, Australia

3.8.7 Sustainability Strategies

The Convention Center presents an opportunity to provide a major green building as well as visible sustainability. The roof of the Convention Center is an opportunity because of its large size, potential environmental impact, and high visibility from adjacent highways, upper levels of buildings, and airplanes flying overhead. See also *Enhancement Opportunity: Convention Center Roof* on the next page.

In addition to integrating the Convention Center within an urban fabric and vertically stacking uses to minimize the overall footprint, there are several current “best practices” relevant to Willets Point.

These “best practices” are:

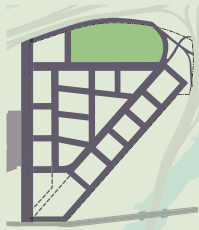
- Energy efficiency through day-lit lobbies and function spaces; energy-efficient building systems and controls; and the use of green power.
- Water conservation through the collection, filtration, and reuse of rainwater. Rain gardens and waterfalls can also be a major public amenity.
- Material conservation through the use of recycled materials and recycling of construction waste.
- Indoor air quality through monitoring and natural ventilation.
- Sustainable food service through the use of local and/or organic produce; sustainable packaging; and commercial food composting.
- Sustainable transportation through the provision of buses, subways, and bicycle support facilities, as well as being within walking distance to hotels.
- Community support through the donation of excess food to local charities and office supplies to local schools.

3.8 CONVENTION CENTER

ENHANCEMENT OPPORTUNITY

Convention Center Roof

The roof of the Convention Center is the single largest building surface in the entire District. It presents an incredible opportunity for use as a green roof for stormwater management, a public garden, or recreational athletic fields. The roof could be a major amenity for the neighborhood.



KEY PLAN

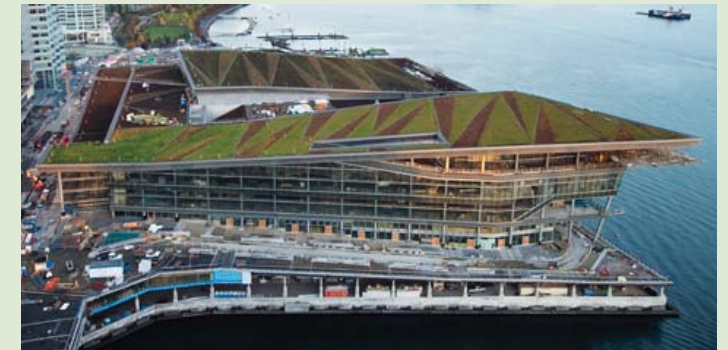


Figure 7. Illustrative view of the Convention Center roof use as athletic fields

The above image depicts the intent of the design guidelines and is for illustrative purposes only



Union City High School, New Jersey



Convention Center, Vancouver

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APPENDIX Guidelines Matrix

1.1 TRANSIT-ORIENTED DEVELOPMENT
 1.2 A CONNECTED NEIGHBORHOOD
 1.3 HIGH DENSITY MIXED-USE DISTRICT
 1.4 LINKED NETWORK OF STREETSCAPES & OPEN SPACES
 1.5 SUSTAINABLE WATER MANAGEMENT
 1.6 AN ENERGY-EFFICIENT DISTRICT
 1.7 GREEN ARCHITECTURE
 1.8 HEALTHY ENVIRONMENT

2 Design Quality Standards

PUBLIC REALM		1.1	1.2	1.3	1.4	1.5	1.6	1.7	1.8
2.1	Pedestrian-Oriented Streets and Sidewalks	●	●		●	●			●
2.2	Parks and Plazas		●		●	●			●
2.3	Trees and Plantings				●	●		●	●
2.4	Hardscape Materials	●	●		●	●			●
2.5	Street Furniture and Lighting	●	●		●		●		●
2.6	Grade Changes: Steps & Ramps								●
ARCHITECTURAL QUALITY		1.1	1.2	1.3	1.4	1.5	1.6	1.7	1.8
2.7	Lower Base: Creating an Active Pedestrian Environment	●	●	●	●			●	●
2.8	Upper Base & Mid-rise: Ensuring Variety with Massing & Articulation	●		●				●	
2.9	Towers: A Varied and Interesting Skyline			●			●	●	
2.10	Roof Design			●		●	●	●	●
2.11	Wall and Window Design			●			●	●	
2.12	Signage Design			●			●		
2.13	Special Use Buildings	●		●		●	●	●	
2.14	Overall Design Quality	●	●	●	●	●	●	●	●

3 Subarea Design Guidelines

AREA A

3.1 126TH STREET

	1.1	1.2	1.3	1.4	1.5	1.6	1.7	1.8
3.1.1	Lower Sidewalk: Part of the NYC Public Street System	●	●		●			●
3.1.2	Transition Zone: Graceful Grade Change				●			●
3.1.3	Upper Zone: Outdoor Dining, Shopping and Strolling	●	●	●	●			●
3.1.4	Open and Inviting Corner Treatments				●			●
3.1.5	Two-Block Core Opposite Citi Field	●	●	●	●			
3.1.6	Emphasize Verticality of Towers			●				●
3.1.7	Respond to Citi Field Massing		●	●				●
3.1.8	Gateways at Connector Streets		●	●				
3.1.9	Spectacular Facade Features that Screen Parking	●		●				●

3.2 RETAIL STREETS

	1.1	1.2	1.3	1.4	1.5	1.6	1.7	1.8
3.2.1	Sidewalk Furnishing Area for Retail Streets	●		●	●			●
3.2.2	Storefront Activity Area	●		●				●
3.2.3	Pedestrian Amenity Zone				●			●
3.2.4	Pedestrian-Oriented Intersections and Crossings	●	●		●			●
3.2.5	Planted Median	●			●	●		●
3.2.6	Active Retail Lower Base	●		●				●
3.2.7	Upper Base Creates Consistent Streetwall			●				●
3.2.8	Mid-rise Articulated from Upper Base			●				●
3.2.9	Towers Setback from Mid-rise	●		●				●

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3.3 ANCHOR BLOCKS		1.1	1.2	1.3	1.4	1.5	1.6	1.7	1.8
3.3.1	Entry Plaza	●	●		●	●			●
3.3.2	Pedestrian Corridor	●			●	●			●
3.3.3	Central Retail Plaza	●			●	●			●
3.3.4	Roosevelt Avenue Greenway	●	●		●	●			●
3.3.5	Signature Towers	●		●				●	
3.3.6	Framing the Pedestrian Gateway	●		●				●	
3.3.7	Building Base: Perimeter Facades	●	●	●				●	●
3.3.8	Building Base: Interior Facades	●		●				●	●
3.3.9	Visual Termini to the Primary Retail Street				●			●	●
3.3.10	Daylit Atria	●		●			●	●	●
3.4 CONNECTOR STREETS		1.1	1.2	1.3	1.4	1.5	1.6	1.7	1.8
3.4.1	District Avenues	●	●	●	●				●
3.4.2	Streetscape Continuity	●	●		●				●
3.4.3	Pedestrian-oriented Retail and Transition Blocks	●		●	●				●
3.4.4	Green Residential Blocks	●			●	●			●
3.4.5	District Entrance: Eastern Gateway		●		●	●			●
3.4.6	Welcoming Gateways	●	●	●				●	
3.4.7	Active Retail and Transition Blocks	●		●				●	●
3.4.8	Area B Neighborhood Corner Retail			●					●
3.4.9	Animated Residential Buildings			●				●	

AREA B

1.1 TRANSIT-ORIENTED DEVELOPMENT
 1.2 A CONNECTED NEIGHBORHOOD
 1.3 HIGH DENSITY MIXED-USE DISTRICT
 1.4 LINKED NETWORK OF DISTRICTS
 1.5 SUSTAINABLE WATER MANAGEMENT
 1.6 AN ENERGY-EFFICIENT DISTRICT
 1.7 GREEN ARCHITECTURE
 1.8 HEALTHY ENVIRONMENT

3.5 RESIDENTIAL STREETS		1.1	1.2	1.3	1.4	1.5	1.6	1.7	1.8
3.5.1	Animated Streets	●			●	●			●
3.5.2	Sustainable Streetscapes				●	●			●
3.5.3	Multiple Entrances on the Ground Floor	●						●	●
3.5.4	Animated Lower Base	●						●	●
3.5.5	Planted Setback Areas				●	●		●	●
3.5.6	Varied Streetwall-Upper Base			●			●	●	●
3.6 NEIGHBORHOOD PARK		1.1	1.2	1.3	1.4	1.5	1.6	1.7	1.8
3.6.1	Sustainable Landscape Identity				●	●			●
3.6.2	Balance of Active and Passive Spaces				●				●
3.6.3	Permeable Edges and View Corridors	●			●				●
3.6.4	Buildings and Structures							●	●
3.6.5	Links to School and Community Facilities			●	●				●
3.7 GREEN EDGES		1.1	1.2	1.3	1.4	1.5	1.6	1.7	1.8
3.7.1	Perimeter Landscape Buffer		●		●	●			●
3.7.2	Welcoming Eastern Gateway		●		●	●			●
3.7.3	Curb cuts for Parking and Service	●		●					●
3.7.4	Roadway and Parking Lane	●	●						●
3.7.5	Active Ground Floor Character	●		●				●	●
3.7.6	Integrated Parking Garages	●		●				●	
3.7.7	Varied Skyline	●	●	●				●	
3.8 CONVENTION CENTER		1.1	1.2	1.3	1.4	1.5	1.6	1.7	1.8
3.8.1	Convention Center Plaza	●		●	●	●			●
3.8.2	Vehicular Access and Drop-off Zone	●		●					●
3.8.3	Landscape Buffer along Northern Boulevard		●	●	●	●			●
3.8.4	Urban Integration	●	●	●					●
3.8.5	Transparent and Articulated Facade	●		●	●			●	●
3.8.6	Public Programming	●		●					●
3.8.7	Sustainable Strategies			●	●	●	●	●	●

ACKNOWLEDGMENTS

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INTERAGENCY WORKING GROUP

New York City Economic Development Corporation
New York City Department of City Planning
New York City Department of Housing, Preservation and Development
New York City Department of Parks and Recreation

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